



Oregon

Theodore R. Kulongoski, Governor

Department of Land Conservation and Development

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NOTICE OF ADOPTED AMENDMENT

02/14/2011

TO: Subscribers to Notice of Adopted Plan
or Land Use Regulation Amendments

FROM: Plan Amendment Program Specialist

SUBJECT: Clackamas County Plan Amendment
DLCD File Number 006-10

The Department of Land Conservation and Development (DLCD) received the attached notice of adoption. Due to the size of amended material submitted, a complete copy has not been attached. A Copy of the adopted plan amendment is available for review at the DLCD office in Salem and the local government office.

Appeal Procedures*

DLCD ACKNOWLEDGMENT or DEADLINE TO APPEAL: Wednesday, March 02, 2011

This amendment was submitted to DLCD for review prior to adoption pursuant to ORS 197.830(2)(b) only persons who participated in the local government proceedings leading to adoption of the amendment are eligible to appeal this decision to the Land Use Board of Appeals (LUBA).

If you wish to appeal, you must file a notice of intent to appeal with the Land Use Board of Appeals (LUBA) no later than 21 days from the date the decision was mailed to you by the local government. If you have questions, check with the local government to determine the appeal deadline. Copies of the notice of intent to appeal must be served upon the local government and others who received written notice of the final decision from the local government. The notice of intent to appeal must be served and filed in the form and manner prescribed by LUBA, (OAR Chapter 661, Division 10). Please call LUBA at 503-373-1265, if you have questions about appeal procedures.

*NOTE: The Acknowledgment or Appeal Deadline is based upon the date the decision was mailed by local government. A decision may have been mailed to you on a different date than it was mailed to DLCD. As a result, your appeal deadline may be earlier than the above date specified. NO LUBA Notification to the jurisdiction of an appeal by the deadline, this Plan Amendment is acknowledged.

Cc: Shari Gilevich, Clackamas County
Gloria Gardiner, DLCD Urban Planning Specialist
Jennifer Donnelly, DLCD Regional Representative
Matt Crall, DLCD Transportation Planner
Gary Fish, DLCD Transportation Planner
Thomas Hogue, DLCD Regional Representative

<paa> YA



FORM 2

DLCD

Notice of Adoption

This Form 2 must be mailed to DLCD within **5-Working Days after the Final Ordinance is signed** by the public Official Designated by the jurisdiction and all other requirements of ORS 197.615 and OAR 660-018-000

In person electronic mailed

DATE STAMP

DEPT OF LAND CONSERVATION AND DEVELOPMENT

FEB 10 2011

For Office Use Only

Jurisdiction: **Clackamas County**

Local file number: **ZDO-226**

Date of Adoption: **February 3, 2011**

Date Mailed: **February 9, 2011**

Was a Notice of Proposed Amendment (Form 1) mailed to DLCD? Yes No Date: April 5, 2010

Comprehensive Plan Text Amendment

Comprehensive Plan Map Amendment

Land Use Regulation Amendment

Zoning Map Amendment

New Land Use Regulation

Other:

Summarize the adopted amendment. Do not use technical terms. Do not write "See Attached".

Adopted boundary for a station community near the Fuller Road light rail station and park-and-ride. Adopted new plan designation and zone district, Station Community Mixed Use, for approximately 26 acres in station community to support office, retail and residential uses near the station. Adopted form-based code emphasizing building type and building orientation to the street to create an active streetscape. Amended existing Corridor Commercial zone (applied only to those parcels, approx. 47 acres, in the station community boundary) to support transition to transit oriented development and improve pedestrian and bicycle connections. Established a walkable block pattern for future redevelopment, and new standards for street cross-sections to include bike lanes, on-street parking, and wide sidewalks to enhance public space for pedestrians.

Does the Adoption differ from proposal? Please select one

- 1) Changed "master plan" section (for commercial development between 82nd Ave & Fuller Rd) to "development threshold" for two sectors. Added Figure to identify existing connections and future required connections for this area.
- 2) For all residential development in new SCMU zone, increased minimum density to 20 units per acre and clarified requirement for onsite, outdoor recreational space. Changed development site minimums to 100-foot street frontage and 0.50 acre site size. Added sign code to prohibit pole signs, message center and changeable copy signs, and add standards for on-building projecting signs.
- 3) Made formatting and word changes to clarify sections and ensure consistency in terms with other sections of the zoning ordinance.

Plan Map Changed from: **Low Traffic Impact Commercial** to: **Station Community Mixed Use**

Zone Map Changed from: **Low Traffic Impact Commercial** to: **Station Community Mixed Use**

Location: **South of SE Johnson Creek Blvd, between SE Fuller Road and I-205**

Acres Involved: **26 acres plan/zone amendment (not including public right-of-way)**

Specify Density: Previous: **Existing, single family houses in LTIC, though not residential zone.**

New: **Minimum residential of 20 units/acre.**

Applicable statewide planning goals:

- | | | | | | | | | | | | | | | | | | | |
|-------------------------------------|-------------------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|-------------------------------------|--------------------------|-------------------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|
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Was an Exception Adopted? YES NO

Did DLCD receive a Notice of Proposed Amendment...

DLCD File no. 006-10 (18229) [16508]

45-days prior to first evidentiary hearing?

Yes No

If no, do the statewide planning goals apply?

Yes No

If no, did Emergency Circumstances require immediate adoption?

Yes No

DLCD file No. _____

Please list all affected State or Federal Agencies, Local Governments or Special Districts:

ODOT; Metro; TriMet; City of Happy Valley; City of Milwaukie

Local Contact: **Shari Gilevich**

Phone: (503) 742-4523 Extension:

Address: **150 Beaver Creek Road**

Fax Number: **503-742-4550**

City: **Oregon City**

Zip: **97045**

E-mail Address: **sharig@co.clackamas.or.us**

ADOPTION SUBMITTAL REQUIREMENTS

This Form 2 must be received by DLCD no later than 5 days after the ordinance has been signed by the public official designated by the jurisdiction to sign the approved ordinance(s) per ORS 197.615 and OAR Chapter 660, Division 18

1. This Form 2 must be submitted by local jurisdictions only (not by applicant).
2. When submitting the adopted amendment, please print a completed copy of Form 2 on light **green paper if available**.
3. Send this Form 2 and one complete paper copy (documents and maps) of the adopted amendment to the address below.
4. Submittal of this Notice of Adoption must include the final signed ordinance(s), all supporting finding(s), exhibit(s) and any other supplementary information (ORS 197.615).
5. Deadline to appeals to LUBA is calculated **twenty-one (21) days** from the receipt (postmark date) of adoption (ORS 197.830 to 197.845).
6. In addition to sending the Form 2 - Notice of Adoption to DLCD, please also remember to notify persons who participated in the local hearing and requested notice of the final decision. (ORS 197.615).
7. Submit **one complete paper copy** via United States Postal Service, Common Carrier or Hand Carried to the DLCD Salem Office and stamped with the incoming date stamp.
8. Please mail the adopted amendment packet to:

**ATTENTION: PLAN AMENDMENT SPECIALIST
DEPARTMENT OF LAND CONSERVATION AND DEVELOPMENT
635 CAPITOL STREET NE, SUITE 150
SALEM, OREGON 97301-2540**

9. **Need More Copies?** Please print forms on **8½ -1/2x11 green paper only if available**. If you have any questions or would like assistance, please contact your DLCD regional representative or contact the DLCD Salem Office at (503) 373-0050 x238 or e-mail **plan.amendments@state.or.us**.

ORDINANCE NO. ZDO-226

An Ordinance amending Chapters 4, 5 and 10 of the Clackamas County Comprehensive Plan and Sections 1000 and 1700 of the Clackamas County Zoning and Development Ordinance and changing the Comprehensive Plan designation and zoning for certain properties in the Fuller Road Station Community

WHEREAS, the Green Line of the metro region’s light rail system opened in September 2009, locating a light rail station and park-and-ride lot east of Fuller Road, between SE Johnson Creek Boulevard and SE Otty Road; and

WHEREAS, Clackamas County deems it essential to increase opportunities to use the light rail system by identifying a station community that will support transit-oriented development; and

WHEREAS, Clackamas County developed a form-based code to support transit-oriented, mixed used development; and

WHEREAS, the amendments are consistent with the Statewide Planning Goals and Guidelines and the Metro Urban Growth Management Functional Plan; and

WHEREAS, after a duly-noticed public hearing, the Clackamas County Planning Commission recommended approval of ZDO-226 on June 14, 2010; and

WHEREAS, the Clackamas County Board of County Commissioners held a public hearing on December 15, 2010; now therefore;

The Board of Commissioners of Clackamas County ordains as follows:

Section 1: The text of Chapters 4, 5 and 10 of the Clackamas County Comprehensive Plan is hereby amended as shown in Exhibit A hereto.

Section 2: The Comprehensive Plan designation is hereby changed from Low Traffic Impact Commercial to Station Community Mixed Use and a corresponding zone change is hereby made from Low Traffic Impact Commercial to Station Community Mixed Use, as shown in Exhibit B hereto, for the following tax lots: T1S, R2E, Section 28BC, Tax Lots 3100, 3201, 3600, 3602, 3700, 4000, 4001, 4100, 4200, 4300, 4500, 4600, 4700, 4800, 4900, 5100, 5200, 5300, and 6500; T1S, R2E, Section 28BD, Tax Lots 2700, 2790, 2800, and 2890; T1S, R2E, Section 28CB, Tax Lots 200, 290, 300, 500, 600, 700, 800, 900, 901, 902, 1000, 1200, 1300, 1600, 1601, 1602, 1700, 1800, 1900, 2000, 2100, 2200, and 2300; T1S, R2E, Section 28CC, Tax Lot 100; and T1S, R2E, Section 28CD, Tax Lot 1100.

Clackamas County Comprehensive Plan Map IV-6 and the official zoning map are hereby amended to reflect the aforementioned change.

Section 3: Clackamas County Comprehensive Plan Maps IV-8 (add Station Community), X-CRC-1 (add Station Community), X-CRC-2 (change the plan designation for the area designated "Low Traffic Impact Commercial" to "Station Community Mixed Use"), X-CRC-4 (add a "new local street location to be determined" on the east side of 82nd Avenue across from Glencoe Road), X-CRC-5 (add a "local street location to be determined" on the east side of 82nd Avenue across from Glencoe Road), X-CRC-6 (add the existing light rail alignment along Interstate 205, two existing light rail stations, and two existing park-and-ride lots, and delete the previously proposed light rail alignment along Harmony Road), and X-CRC-7 (add a "proposed bikeway" to Fuller Road, from Otty Road to Con Battin Road, and to Con Battin Road as shown on the revised map, and add two existing locations of connections to the Interstate 205 multi-use trail) are hereby amended as shown in Exhibit C hereto.

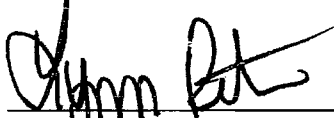
Section 4: New Figures X-CRC-8 through X-CRC-11 are hereby adopted as part of the Clackamas County Comprehensive Plan as shown in Exhibit D hereto.

Section 5: Sections 1000 and 1700 of the Clackamas County Zoning and Development Ordinance are hereby amended as shown in Exhibit E hereto.

Section 6: This ordinance shall be effective on March 7, 2011.

ADOPTED this 3rd day of February, 2011

BOARD OF COUNTY COMMISSIONERS


Chair

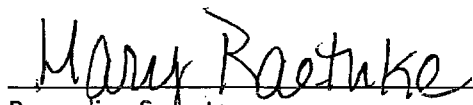

Recording Secretary

EXHIBIT A
Ordinance No. ZDO-226
Clackamas County Comprehensive Plan Amendments

Text to be added is underlined. Text to be deleted is ~~struck through~~.

The following is excerpted from Chapter 4 (Land Use) of the Comprehensive Plan. No changes are made to any other section of Chapter 4.

URBAN GROWTH CONCEPT

This section of the Land Use Chapter addresses the implementation of the Region 2040 Growth Concept as it applies to Clackamas County. It provides for design type areas that are consistent with the general locations shown on the Region 2040 Growth Concept Map.

Clackamas County, with approximately 67% of its population inside the Portland Metropolitan Urban Growth Boundary, is a partner in the region's efforts to efficiently utilize the land inside the boundary. This will minimize the need to expand the boundary and protect the land available for agricultural, forest and rural uses. The intent of the Urban Growth Concept is to focus increased development in appropriate locations, such as existing commercial centers and along transportation corridors with existing or planned high quality transit service. It also encourages increased employment densities in industrial and employment areas.

The provisions of the Urban Growth Concept apply in addition to other requirements identified in the Clackamas County Comprehensive Plan. The Urban Growth Concept is designed to provide guidance for Comprehensive Plan and Zoning Development Ordinance changes, as well as to identify specific development review requirements. All provisions except Green Corridors apply to lands inside the Portland Metropolitan Urban Growth Boundary. Green Corridors apply to rural, agricultural and forest areas. Future Urban Study Areas are areas in transition. When concept planning is completed for these areas, growth concept design types will be adopted as appropriate.

DEFINITIONS

Growth Concept Design Types

The locations of the following design types are identified on the Clackamas County Urban Growth Concept Map: (Map IV-8) or as described below: (5/27/04)

Regional Center: An area that is the focus of compact development,

redevelopment, high quality transit service and multi-modal street networks. The intent of the Regional Center is to provide an area for the most intense development and highest densities of employment and housing.

Corridors: Areas located along streets which have existing or planned high quality transit service and feature a high quality pedestrian environment, convenient access to transit and increased residential and employment densities. The intent of the Corridor designation is to encourage increased densities by facilitating zone and plan changes in specific locations. In addition, it provides guidance for development review to implement a high quality pedestrian environment.

The streets where the Corridor design type designation is applied are: McLoughlin Blvd. (from Milwaukie to Gladstone), 82nd Avenue (within the Clackamas Regional Center Design Plan Area), Johnson Creek Boulevard (within the Clackamas Regional Center Design Plan Area), and Sunnyside Road (from 82nd Avenue to 139th Avenue).

Station Community: Areas centered around a light-rail or high capacity transit station that feature housing, offices and other employment, and a variety of shops and services that are easily accessible to pedestrians, bicyclists and transit users as well as vehicles. The intent of the Station Community designation is to encourage transit oriented development with a mix of high density housing and employment uses, a high quality pedestrian environment and other features designed to encourage high transit ridership.

Employment areas: Areas providing for various types of employment and some residential development with limited large-scale commercial uses. The intent is to define the appropriate locations for “big box retailers” to allow for more employment intensive uses.

Industrial areas: Areas set aside primarily for industrial activities with limited supporting uses. The intent is to prohibit “big box retailers” from these areas to allow for industrial uses.

Neighborhoods: Primarily residential areas that are accessible to jobs and neighborhood businesses. This broad category includes areas set aside for homes, parks and open space, schools, public services, and neighborhood business uses. The intent is to facilitate the Region 2040 “Inner Neighborhood” design type.

Green Corridors: Areas outside the Urban Growth Boundary adjacent to major transportation routes to neighboring cities where the rural character of the landscape and agricultural economy shall be maintained. The intent is to preserve the view sheds and maintain the rural character between urban areas along the major transportation routes.

Future Urban Study Areas: Areas brought within the Urban Growth Boundary for which the required planning has not yet been completed. The intent is to identify the areas where Title 11 of the Urban Growth Management Functional Plan and Metro code specify that concept planning and other requirements must be completed before other Urban Growth Concept design types and urban plan designations can be applied. Future Urban Study Areas include areas identified on Map IV-8 and areas brought into the Portland Metropolitan UGB after the adoption of Map IV-8. (5/27/04)

GOALS

- Provide for a compact urban form, integrating the built environment, transportation network, and open space, that:
 - Minimizes the amount of Urban Growth Boundary expansion required to accommodate expected population and employment growth in the next 20 years.
 - Efficiently uses public services including transportation, transit, parks, schools, sewer and water.
 - Distinguishes areas for intensive development from areas appropriate for less intensive development.
 - Preserves existing stable and distinct neighborhoods by focusing commercial and residential growth in mixed use centers and corridors.
 - Develops mixed use centers and corridors at a pedestrian scale and with design features and public facilities that support pedestrian, bicycle and transit trips.
- Maintain the rural character of the landscape between the Urban Growth Boundary and neighboring cities.

POLICIES

Regional Center

- 1.0 The Regional Center design type designation is applied to the Clackamas Regional Center, as identified on Map IV-8. The goals and policies applicable to the Clackamas Regional Center are located in Chapter 10: Clackamas Regional Center Area Design Plan.

Corridors

- 2.0 The "Corridor Design Type Area" designation is applied to sites adjoining the Corridor streets shown on Map IV-8. Corridor Design Type Areas may be either continuous or development nodes. The areas of application for Corridor Design Type Areas are specified in Chapter 10 for all of the Corridor Streets.

The Policies that apply to all the Corridor Design Type Areas include:

- 2.1 Provide for both employment and housing, including mixed use.
- 2.2 Provide for a high level of bus usage, with land uses and transportation facilities to support bus use.
- 2.3 Encourage and support pedestrian travel with supportive land uses, frequent street connections, and sidewalks and pedestrian-ways.
- 2.4 Provide for vehicular traffic and auto-oriented uses, while expanding the share of trips via transit and other modes.
- 2.5 Enhance connectivity between neighborhoods adjacent to the Corridor Design Type Area and the Corridor Street.
- 3.0 Specific policies for the SE 82nd Ave, SE Johnson Creek Boulevard and SE Sunnyside Road (from 82nd Ave to approximately SE 117th Ave.) corridors are located in Chapter 10: Clackamas Regional Center Area Design Plan.
- 4.0 Specific policies for the Sunnyside Road (from approximately SE 117th Ave to SE 139th Avenue) Corridor Design Type Area are located within Chapter 10: The Sunnyside Corridor Community Plan.
- 5.0 Specific policies for the McLoughlin ~~Boulevard~~Blvd- Corridor Design Type Area are located in Chapter 10: McLoughlin Corridor Design Plan.

Station Communities

- 6.0 The Station Community designation is applied to sites surrounding a light rail or other high capacity transit station as shown on Map IV-8. The areas of application for Station Community Design Type Areas are specified in Chapter 10. The Policies that apply to all the Station Community Design Type Areas include:
- 6.1 Provide for transit oriented development with land uses that support a high level of transit usage, such as a mix of high density employment and housing uses.

6.2 Provide a high quality pedestrian and bicycle environment with frequent street connections, walkways and bikeways.

6.3 Enhance connectivity between neighborhoods adjacent to the Station Community.

7.0 Specific policies for the Fuller Road Station Community are located in Chapter 10: Clackamas Regional Center Area Design Plan.

Employment Areas

68.0 The Employment Area Design Type designation is applied as shown on Map IV-8. Policies that apply to the Employment Design Type Areas include:

68.1 Employment Design Type Areas shall be developed to provide for a mix of employment and residential uses, including:

- a. Industry, office and service uses,
- b. Residential development,
- c. Low traffic generating, land consumptive commercial uses with low parking demand which have community or region-wide market,
- d. Limited retail uses designed primarily to serve the needs of people working or living in the immediate Employment Design Type Area.

68.2 Retail facilities larger than 60,000 square feet of gross leasable area per building or business may be allowed on sites designated for General Commercial uses in or before 1996, or when allowed by zoning and:

- a. Transportation facilities adequate to serve the retail use, consistent with Metro's functional plan for transportation, will be in place at the time the retail use begins operation; and,
- b. Transportation facilities adequate to meet the transportation need for other planned uses in the Employment Design Type Area are also provided.

Industrial Areas

79.0 The Industrial Area Design Type designation is applied as shown on Map IV-8. Policies that apply to the Industrial Areas include:

79.1 Retail uses larger than 60,000 square feet of gross leasable area per building or business are prohibited.

Neighborhoods

810.0 The Neighborhood Design Type designation is applied as shown on Map IV-8. Policies that apply to the Neighborhood Areas include:

810.1. Development of areas planned for residential, commercial and industrial uses within Neighborhood Design Type Areas shall be guided by the urban land use policies outlined in the Land Use Chapter in the Comprehensive Plan.

810.2 Areas designated as Low Density Residential shall achieve the densities as outlined in the Low Density Residential policy section.

Future Urban Study Areas

811.0 The Future Urban Study Area Design Type designation is applied as shown on Map IV-8. The goals and policies applicable to Future Urban Study Areas are located in the Urbanization section of Chapter 4: Land Use.

Green Corridors

102.0 The goals and policies for Green Corridors shall be defined through a separate study as outlined in the Intergovernmental Agreements on Green Corridor and Rural Reserve and Population Coordination, signed by Clackamas County, City of Sandy, City of Canby, ODOT and Metro.

The following is excerpted from the Roadways section of Chapter 5 (Transportation) of the Comprehensive Plan. No changes are made to any other section of Chapter 5.

Operating Standards

30.0 Evaluate capacity needs for regional roadways within Metro's boundaries using the Regional Motor Vehicle Performance Measures. The use of these measures is limited to network analysis, and priorities for funding through Metro; they are not for designing individual road improvements.

31.0 ~~All arterials~~ Arterials and collectors ~~not in Regional Centers~~ shall be evaluated for performance to Level-of-Service "D" as the acceptable operating standard, except as established below. All capital construction shall be designed to achieve Level-of-Service "D" or better. (4/28/05)

- a. Review of high-employment developments shall use a performance evaluation operating standard of Level-of-Service "E". (4/28/05)
- b. Review of developments proposed on property with a Comprehensive Plan designation of Campus Industrial, Business Park, Light Industrial, General Industrial or Rural Industrial shall use a performance evaluation operating standard of Level-of-Service "E", except within the Clackamas Industrial Area and Government Camp Village where no performance evaluation operating standard shall apply. (1/25/07)
- c. Segments of 82nd Avenue, Sunnyside Road, and Johnson Creek Boulevard located within the Clackamas Regional Center Area shall be evaluated for performance to standards adopted in Chapter 10, "Clackamas Regional Center Area Design Plan," Section XII Roads and Streets System Policies.

32.0 For state facilities within an Interchange Management Area as identified on Map V-12, implement a mobility standard for the peak two hours of 0.99 v/c at the intersection and of 0.85 v/c at the ramp ends. (11/17/10)

33.0 Limit zone change approvals to those that will not reduce the Level of Service of a roadway as planned in the Capital Improvement Plan below the minimum acceptable performance evaluation Level-of-Service standard. State facilities shall be evaluated according to the Oregon Highway Plan. (3/14/02)

The following is excerpted from Chapter 10 (Community Plans and Design Plans) of the Comprehensive Plan. No changes are made to any other section of Chapter 10.

CLACKAMAS REGIONAL CENTER AREA DESIGN PLAN

INTRODUCTION

Moving Towards a Preferred Future

The Clackamas Regional Center area, comprising about 2,100 acres, is a vital and growing part of the County. It is a major hub for the residential and business communities in the southeast Portland metropolitan area. The area has grown rapidly as urban services have been provided, and is poised for even more growth. Forecasts indicate that there will be 36,500 jobs within the study area and 7,600 housing units by the year 2017. This will about double the amounts present in

1994. As this change occurs over the next twenty years, the area is envisioned to transition to even more intensive uses, more mixes of land uses, better access for all modes of transportation and a more attractive visual character.

The Clackamas Regional Center Area Design Plan sets the framework for decision-making to meet the challenge of planning for growth and guiding the area to a preferred future identified by citizens, the business community, and public service providers.

The overall Clackamas County Comprehensive Plan is applicable to the Clackamas Regional Center Area. This chapter of the Comprehensive Plan describes the goals and policies that are specific to the Clackamas Regional Center Area. This chapter takes precedence where conflicts exist between it and the remainder of the Comprehensive Plan.

The area of application for the Clackamas Regional Center Area Design Plan is shown on Map X-CRC-12.

REGION 2040 GROWTH CONCEPT PLAN DESIGN TYPES

The Clackamas Regional Center Area Design Plan focuses on ~~three~~ two design-types identified in the Region 2040 Growth Concept Plan and Urban Growth Management Functional Plan: a "regional center," and segments of three "corridors," and a "station community."

Regional Center

An area with the Clackamas Town Center as its focus point is designated a "regional center". The boundary is shown on Map X-CRC-1. The Clackamas Regional Center is intended to be the focus of the most intense development and highest densities of employment and housing in unincorporated, urban Clackamas County, with high quality transit service and a multi-modal street network.

Corridors

Corridors are less dense than 'regional centers' and are intended to feature a high quality pedestrian environment and convenient access to transit, while continuing to meet the needs of the automobile. The corridors in the Clackamas Regional Center Design Plan Area are designated "regional streets" in the Region 2040 Functional Plan, and as such are expected to continue to support high levels of through and local vehicular traffic. The corridor areas are expected to transition to higher densities through infill and redevelopment.

Designated corridors are SE 82nd Avenue, Johnson Creek Boulevard, and Sunnyside Road.

Station Community

Station communities are areas of development centered on a light-rail or high capacity transit station that feature housing, offices and other employment, and a variety of shops and services that are easily accessible to pedestrians, bicyclists and transit users, as well as vehicles. There are two light rail transit stations in the I-205 MAX line in the Clackamas Regional Center Design Plan Area; adjacent to I-205 near SE Fuller Road, between SE Johnson Creek Boulevard and SE Otty Road, and adjacent to I-205, between SE Monterey Avenue and SE Sunnyside Road. A Station Community has been designated in the area around the Fuller Road station.

VISION AND GOALS

A Vision of how the area should look and function in 20 years was the first step in creating this plan. The Vision established the foundation upon which the plan was built. The Clackamas Regional Center Area Task Force developed and endorsed the following Vision for the Clackamas Regional Center Area in 1995:

Vision

Over the next 20 to 50 years the Clackamas Regional Center Area will be:

- The dominant commercial and business center for the east Portland metropolitan area;
- A cultural, civic and transportation center for the east Portland metropolitan area;
- An area of diverse residential neighborhoods, commercial districts, natural features, and public attractions and spaces that serve both the local community and the region.

Goals

To achieve this Vision, the Clackamas Regional Center Area Design Plan describes policies to guide decisions on land use, transportation, housing and urban design that:

- Allow and promote compact development as a means to encourage efficient use of land, promote non-auto trips, and protect air quality.
- Promote development patterns which use land efficiently and support transportation investments.

- Transition towards more intensive use of land through infill and redevelopment, and phased development of infrastructure and urban design improvements.
- Accommodate and encourage appropriate land uses in the Regional Center, ~~and~~ along Corridors and in the Station Community.
- Balance growth with the preservation of existing neighborhoods and affordable housing.
- Create districts and neighborhoods.
- Provide a range of housing types and density.
- Provide for more efficient parking.
- Provide or enhance public amenities such as open space, neighborhood parks, and public gathering places.
- Preserve and enhance natural features.
- Increase community attractions.
- Provide attractive streetscapes.
- Create civic spaces.
- Create a safe and pleasant environment.
- Incorporate design standards and guidelines that promote urban character.
- Increase visual identity.
- Provide a transportation network that provides for all modes of transportation.
- Improve circulation and connections for all modes of transportation.
- Maintain excellent regional access.

CLACKAMAS REGIONAL CENTER AREA DESIGN PLAN POLICIES

The following policies shall be applied in the Clackamas Regional Center Design Plan Area.

LAND USE POLICIES

I. LAND USE POLICIES GENERALLY

Map X-CRC-2 illustrates the Land Use Plan designations for the Clackamas Regional Center Design Plan Area. The following uses are allowed:

1.0 Mixed Use

Mixed uses shall be allowed in the Clackamas Regional Center Design Plan Area in areas designated Commercial, High Density Residential and Regional Center High Density Residential. A mix of uses will be required to be master planned in Planned Mixed Use designated areas. A mix of uses will be allowed in Station Community Mixed Use designated areas, subject to transit-oriented-development building orientation and design requirements.

2.0 Commercial

The following primarily retail commercial designations shall be provided in the Clackamas Regional Center Design Plan Area: Regional Center Commercial, Retail Commercial, and Corridor Commercial and Low Traffic Impact Commercial.

The following primarily office commercial designations shall be provided in the Clackamas Regional Center Design Plan Area: Regional Center Office and Office Commercial.

Commercial areas within the Clackamas Regional Center Design Plan Area shall:

- 2.1 Allow a mix of land uses on the development site.
- 2.2 Create a district accessible by all modes of transportation.
- 2.3 Create walkable districts by providing improvements and urban design features that encourage and support pedestrian use:
- 2.4 Allow land uses that generate pedestrian activity and transit ridership
- 2.5 Require public or private street layouts that allow for future development of sites with redevelopment potential.

- 2.6 Maintain and improve pedestrian connections between commercial uses, transit corridors, recreation areas, open space and adjacent residential areas.
- 2.7 Locate all buildings to maximize access by emergency vehicles.
- 2.8 Require Design Review for all development.

3.0 Multifamily Residential

The following primarily multifamily residential designations shall be provided in the Clackamas Regional Center Design Plan area: Regional Center High Density Residential, High Density Residential, Medium High Density Residential, and Medium Density Residential.

Multifamily areas within the Clackamas Regional Center Design Plan Area shall:

- 3.1 Establish minimum densities to help meet local and regional housing needs.
- 3.2 Provide for multifamily residential uses within walking distance of public transportation, parks, schools, employment areas and local shopping areas.
- 3.3 Create walkable districts by providing improvements and urban design features that encourage and support pedestrian use.
- 3.4 Locate all buildings to maximize access by emergency vehicles.
- 3.5 Require design review for all development.

4.0 Public and Community Use, Open Space

Public and Community use designations including open space shall be provided in the Clackamas Regional Center Design Plan Area.

5.0 Low Density Residential

Low density residential designations shall be provided in the Clackamas Regional Center Design Plan area.

6.0 Industrial

The following industrial designations shall be provided in the Clackamas Regional Center Design Plan Area: General Industrial, Light Industrial, and Business Park.

II. LAND USE POLICIES FOR THE CLACKAMAS REGIONAL CENTER DESIGN TYPE AREA

1.0 Within the Regional Center boundary shown on Map X-CRC-1, areas shall be planned to:

- 1.1 Provide for high intensity development to accommodate projected regional increases in housing and employment, including mixed use development.
- 1.2 Provide for and capitalize on high quality transit service.
- 1.3 Allow for a mix of land uses to support public transportation and bicycle and pedestrian usage.
- 1.4 Provide for the open space and recreation needs of residents and employees of the area.
- 1.5 Support a multi-modal street network.

2.0 Planned Mixed Use

The Planned Mixed Use designation allows for master planning and development on key opportunity sites in areas designated for mixed use on the Region 2040 Growth Concept map. Generally, because of size, location, good access, and proximity to supportive land uses and existing or planned transportation improvements, these sites can accommodate more growth than other areas and sites within the plan boundary.

- 2.1 Create an area with a mix of land uses, both within the site itself (mix of uses) and within buildings (mixed uses), which:
 - a. Provide for high employment and residential densities that support use of public transportation.
 - b. Protect key natural features.
 - c. Provide for essential public facilities and services, including parks and public spaces.
 - d. Are accessible by all modes of transportation.

- 2.2 Establish through zoning required and allowed land uses, transportation improvements, and design standards that encourage and support pedestrian-oriented streets, buildings and public places. Apply specific requirements to specific Planned Mixed Use sites through zoning.
- 2.3 Apply the Planned Mixed Use designation within the Regional Center as shown on Map X-CRC-1.
- 2.4 Sites planned for Planned Mixed Use but zoned for other uses may be converted to Planned Mixed Use zoning when:
 - a. Adequate transit services are provided to the site; and,
 - b. Minimum site size requirements are satisfied.

3.0 Regional Center Office

- 3.1 Apply the Regional Center Office designation within the Regional Center boundary shown on Map X-CRC-1 to:
 - a. Areas with an historical commitment to office use.
 - b. Areas served by high capacity transit service.
 - c. Areas with high visibility from a freeway.
 - d. Areas generally within ½ mile of a freeway interchange.
- 3.2 Provide support services for office development.
- 3.3 Limit retail uses in order to maximize the land available for office uses and to provide for the highest employment density in the Regional Center.
- 3.4 Require a minimum density to help meet regional employment needs, support public transportation and use land more efficiently.
- 3.5 Create walkable districts within the regional center with improvements, urban design features, and urban design standards that encourage and support pedestrian use.
- 3.6 Require master plans of large sites to allow for future development of sites with redevelopment potential.

4.0 Regional Center High Density Residential

Within the adopted Regional Center boundary, designate areas suitable for the highest density multifamily uses as Regional Center High Density Residential.

- 4.1 Determine the density of development through zoning.
- 4.2 Provide for multifamily residential uses within walking distance of public transportation, parks, schools, employment areas and local shopping areas.
- 4.3 Allow for a mix of land uses provided the minimum residential density is achieved for the entire development site prior to or concurrent with establishment of other allowed uses.

5.0 Regional Center Commercial

Apply the Regional Center Commercial zone to areas with an historic commitment to commercial uses within the adopted Regional Center boundary as shown on Map X-CRC-1.

- 5.1 Provide areas for regional and local shopping.
- 5.2 Require a minimum floor area ratio to help meet regional employment needs, support public transportation and use land more efficiently.
- 5.3 Create walkable districts within the regional center with improvements, urban design features, and urban design standards that encourage and support pedestrian use.

6.0 Amendments to the Clackamas Regional Center Boundary

The Clackamas Regional Center boundary may be amended to include property within the Clackamas Regional Center when all of the following criteria are met:

- 6.1 The property is contiguous to the Clackamas Regional Center boundary.
- 6.2 The area is, or is planned to be, a focus of compact, high density development with a mix of uses.
- 6.3 The area has, or is planned to have, high quality transit service, and a multi-modal street network.

- 6.4 The area has, or is planned to have; a density of 60 persons per acre on lands developed or planned to be developed (not including open space, parks, plazas or natural areas).

III. LAND USE POLICIES FOR CORRIDOR DESIGN TYPE AREAS

1.0 Land uses in Corridors shall be planned to:

- 1.1 Provide for both employment and housing, including mixed use.
- 1.2 Emphasize providing for a high level of bus usage, with land uses and transportation facilities to support bus use.
- 1.3 Encourage and support pedestrian travel with supportive land uses, frequent street connections, and sidewalks and pedestrian-ways.
- 1.4 Provide for vehicular traffic and auto-oriented uses, while expanding the share of trips via transit and other modes.

2.0 Corridor Land Use Designations

A range of land use designations may be applied within a designated Corridor as shown on Map X-CRC-2 may be designated in corridors as identified on Map X-CRC-1. Each corridor shall include within its area land use designations which provide primarily for employment and shopping, and land use designations that provide primarily for residences.

- 2.1 Commercial designations that may be applied include: Corridor Commercial, Retail Commercial, ~~Low-Traffic Impact Commercial,~~ and Office Commercial. Any site designated for a commercial use shall be located adjacent to the cCorridor sStreet.
- 2.2 Multifamily designations that may be applied include: High Density Residential and Medium High Density Residential. Multifamily designations should generally be located so as to form a buffer between commercial uses adjacent to the corridor street and low density residential areas located outside the corridor.
- 2.3 Industrial designations that may be applied in corridors include: Light Industrial and Business Park.
- 2.4 Existing single family neighborhoods and mobile home parks should be zoned to discourage redevelopment to other uses.

3.0 Corridor Commercial

3.1 The following areas may be designated Corridor Commercial when located within a transportation corridor as identified on Map X-CRC-1 and when all of the following criteria~~æ~~ have been met:

- a. The site has an historical commitment to commercial uses,
- b. The designation will not cause a decrease in housing capacity in the county,
- c. The designation will not cause a significant traffic increase on local streets serving residential areas,
- d. Adverse effects including but not limited to traffic and noise, will have a minimal effect on adjacent neighborhoods or can be minimized through on-site improvements; and
- e. The designation will not substantially increase an existing commercial strip or create new strips.

3.2 Provide commercial areas located in transportation corridors to meet local and regional needs for a wide range of goods and services.

3.3 Provide for the sale of large-scale items in areas with good transportation access and minimal conflict with other uses.

3.4 Allow mixed uses in the same building(s) or in a separate building(s) in the development.

3.5 Establish design and dimensional standards that encourage and support pedestrian use.

**IV. LAND USE POLICIES FOR THE STATION COMMUNITY
DESIGN TYPE AREA**

1.0 The Regulating Plan Map, which will be incorporated in the Zoning and Development Ordinance, shall be the basis of the design and development standards for the Station Community and shall establish the requirements for street types, block pattern, existing and new streets, building frontage types, and landscaping types.

2.0 Within the Station Community boundary shown on Map X-CRC-1, future development and redevelopment shall conform to the Regulating Plan Map, and areas shall be planned to:

- 2.1 Provide for development utilizing urban design elements that create and support a dynamic, safe and convenient public realm made up of inter-connected streets, parking areas, parks and plazas framed by buildings with facades and entrances facing the streets and meeting other requirements of transit oriented design.
- 2.2 Provide for a mix of retail, services, office and high intensity housing in buildings meeting the requirements of transit oriented design, located on a street network with excellent pedestrian connectivity and supportive of local services, bicycle and pedestrian usage, and high capacity transit ridership.
- 2.3 Support a multi-modal street network with shared, public on-street parking on all but the most heavily traveled streets, building facades and entrances oriented to the street, and parking located to the side and behind buildings.
- 2.4 Provide for the open space and recreation needs of residents and employees of the area.

3.0 Corridor Commercial

- 3.1 Apply the Corridor Commercial designation within the Station Community boundary shown on Map X-CRC-1 to:
 - a. Areas with an historical commitment to retail uses.
 - b. Areas with high visibility and access from a major arterial street.
 - c. Areas located within ½ mile of a high capacity transit station, and providing actual or potential pedestrian connections between high capacity and bus transit.
- 3.2 Create an area with a mix of land uses, both within the site itself (mix of uses) and within buildings (mixed uses), which:
 - a. Provide for high employment and residential densities that support use of public transportation.
 - b. Provide for essential public facilities and services, including shared public parking on public and private streets, accessible and attractive walkways between and through developments, and public spaces.

- c. Are accessible by all modes of transportation.
- d. Orient buildings and parking areas to support and encourage pedestrian trips and utilization of high capacity transit.

3.4 Establish through zoning required and allowed land uses, transportation improvements, and design standards that encourage and support pedestrian-oriented streets, buildings, and public places.

- a. Require development and redevelopment to meet transit-oriented design requirements.

3.5 In designated sectors on the Regulating Plan Map, where substantial shopping center development exists, provide for a limited amount of redevelopment to occur without requiring full compliance with transit-oriented design and connectivity requirements.

- a. Ensure that such redevelopment does not reduce multimodal connectivity or hinder future development of additional planned connections.

4.0 Station Community Mixed Use

4.1 Apply the Station Community Mixed Use designation within the Station Community boundary shown on Map X-CRC-1 to:

- a. Areas with an historical commitment to residential, office and employment uses.
- b. Areas in proximity to high capacity transit service.
- c. Areas with access to major and minor arterial and collector streets.

4.2 Create an area with a mix of residential, office, service and service commercial uses within buildings and developments that meet transit oriented development standards, which:

- a. Provide for high employment and residential densities that support use of public transportation.
- b. Provide for essential public facilities and services, including shared public parking on public and private streets.

accessible and attractive walkways between and through developments, and public spaces.

c. Orient buildings and parking areas to support and encourage pedestrian trips and utilization of high capacity transit.

4.3 Establish through zoning required and allowed land uses, transportation improvements, and design standards that encourage and support pedestrian-oriented streets, buildings, and public places.

a. Require development and redevelopment to meet transit-oriented design requirements.

5.0 Build public and private streets within the Station Community to the standards illustrated in the Street Type cross sections (Figures X-CRC-8 through X-CRC-11).

6.0 Study providing on-street parking on 82nd Avenue, if future conditions warrant it.

IV. LAND USE POLICIES FOR OTHER AREAS WITHIN THE CLACKAMAS REGIONAL CENTER DESIGN PLAN AREA

1.0 A range of land use designations shall be provided in portions of the Clackamas Regional Center Design Plan Area located outside the Regional Center, ~~and~~ Corridors, and Station Community.

1.1 Land use designations shall generally increase in level of intensity in areas close to the Regional Center and Corridors.

1.2 Land use designations shall maintain the character of existing neighborhoods by providing for uses and improvements that are consistent with the type and scale of existing development.

1.3 Employment uses shall be provided for in the Regional Center, ~~or~~ Corridors, or Station Community, and/or in locations adjacent to streets that are at least minor arterials.

VI. LAND USE POLICIES: LAND USE DESIGNATIONS THAT MAY APPLY THROUGHOUT THE CLACKAMAS REGIONAL CENTER DESIGN PLAN AREA

~~4.0 Low Traffic Impact Commercial~~

~~4.1 Areas may be designated Low Traffic Impact Commercial when~~

~~both of the following criteria are met:~~

- ~~a. The area is located on at least a minor arterial street, and~~
- ~~b. The area has excellent visibility where uses with low peak hour traffic generation are necessary to help reduce the traffic impacts on inadequate intersections.~~

~~1.2 Allow uses with low peak hour traffic generation as specified in the Institute of Traffic Engineers Trip Generation Manual or other professionally recognized resources.~~

~~1.3 Maintain and improve automobile and pedestrian connections between commercial uses, transit corridors, recreation areas, open space and adjacent residential areas.~~

~~1.4 Require street layouts that provide for future development of the site or adjacent property.~~

~~1.5 Areas designated Low Traffic Impact Commercial may be considered for other zones when sufficient capacity is added to the street system to accommodate higher peak volume traffic loads.~~

~~1.6 Require design review for all development.~~

| 12.0 High Density Residential

In the High Density Residential district, allow for a mix of land uses as a limited use.

| 23.0 Low Density Residential – 5,000 and 2,500 square foot lots

In the Low Density Residential district, include 5,000 square foot and 2,500 square foot lot size low density residential zones, subject to Policy 2.0 of the Land Use Chapter, Residential Section of the Comprehensive Plan.

| 34.0 Low Density Residential – Single Family Attached

| 34.1 In Low Density Residential areas, areas may be zoned for single family attached residences on lots that average 2,500 square feet when the area has access to a residential collector or higher functional class street.

| 34.2 The size of the site and adjoining properties zoned for 2,500 square foot lots should generally not exceed ten (10) acres. Sites greater

than 10 acres must include a combination of attached and detached housing within the allowed Single Family Attached density.

| 34.3 Design dwellings to provide variation in architectural appearance.

| 34.4 Require Design Review for single family attached residences.

URBAN DESIGN, PUBLIC AMENITIES, AND OPEN SPACE POLICIES

Design and development standards and physical improvements tie together land use and transportation to create a more “livable” community. Urban Design elements have been identified that will improve access by all modes of transportation; provide public amenities such as parks and accessible trails for recreational use; create public gathering places, and protect key natural features such as stream corridors and forested hillsides:

VII. URBAN DESIGN ELEMENTS

- 1.0 Establish design and dimensional standards that provide pedestrian oriented streets, buildings and public spaces.
- 2.0 Provide for the most intense development around public transportation routes.
- 3.0 Provide multi-modal connections that link neighborhoods with commercial areas, schools, parks and greenways.
- 4.0 Increase the visual identity of the Regional Center Area through streetscape improvements including pedestrian zones, landscaped strips between streets and sidewalks, lighting, street trees, landscaped medians, and gateways.
- 5.0 Protect natural features by directing development away from these areas and using remaining land more efficiently.
- 6.0 Provide public or private street layouts that support future development and increase connectivity for all modes of transportation.
- | 7.0 The Urban Design Elements shown on Map X-CRC-3 shall be provided in the Clackamas Regional Center Design Plan Area as development occurs and public improvements are provided.
 - 7.1 All new development or major modifications to existing approved development shall provide the design elements on Map X-CRC-3.

- 7.2 For phased development, urban design requirements will generally be roughly proportional to the amount of development occurring in a phase.
- 7.3 Key urban design elements shown on Map X-CRC-3 are defined as follows:
- a. Boulevards: Streets characterized by landscaped medians and other pedestrian crossing improvements, a sidewalk separated from the street by planting strips and street trees, and bike lanes.
 - b. Main Streets: Streets characterized by a pedestrian/furnishing zone that includes sidewalks, street trees, and space for street lights and other furnishings, on-street parking, more frequent pedestrian crossings, and buildings oriented to the street with storefronts close to the sidewalk.
 - c. Special Street Standards: Streets that are characterized by a landscaped planting strip separating the sidewalk from the curb, pedestrian lighting, and pedestrian amenities.
 - d. Street Connections: General locations for new or enhanced street connections to improve connectivity in the area have been identified on Map X-CRC-3. Street connections may be public or private streets and in some cases line up with important driveways to commercial areas.
 - e. Local Street Grid: An interconnected public or private street system that provides multi-modal access to all activities and uses.
 - f. Off-street Pedestrian Linkages: Street, bicycle and pedestrian paths, and greenway paths to link parks, civic spaces, retail centers, neighborhoods, and other points of interest.
 - g. Multi-Use Paths: Off-street pedestrian and bicycle paths. These paths may be developed primarily as a transportation facility, as an amenity, or may serve multiple purposes.
 - h. Parks and Open Space: The general locations of parks needed in the Clackamas Regional Center Design Plan area are shown on the Map X-CRC-3. Park locations are not site-

specific.

- i. Greenway Trails: Off-street trails within designated greenways (e.g. Phillips Creek and Mt. Scott Creek) that provide opportunities for environmental restoration, recreation and education.
- j. Plazas: Public gathering places are typically one acre or less and may be publicly or privately owned. Plazas are intended as public gathering places and community focal points.
- k. Natural Features: Natural features to be protected include creeks, wetlands, steep slopes and wooded bluffs.
- l. Gateways: Key intersections to be reconstructed with special design and landscape treatments that are intended to provide a visual announcement that people are entering a special area.

8.0 Establish though zoning transit-oriented design standards to ensure that streets and buildings are supportive of pedestrian, bicycle, and transit trips.

VIII. STREETS AND GATEWAYS

- 1.0 Establish design and dimensional standards that provide pedestrian oriented streets and buildings.
- 2.0 Design and dimensional standards for streets and gateways are intended to:
 - a. Improve pedestrian safety at crossings.
 - b. Improve visual appeal of the streets.
 - c. Improve the pedestrian environment along sidewalks.
 - d. Provide on-street parking where appropriate to help provide a supply of public parking that supports reduced parking standards on private property, and separate pedestrians from auto traffic.
 - e. Provide strong visual identity to distinguish the Regional Center from adjacent areas.
 - f. Create a local block pattern for new roads to improve circulation for motor vehicles and pedestrians by providing shorter and more direct connections between uses.

- 3.0 Boulevards, Main Streets, Gateways, and streets planned for Special Street Standards have been identified on Map X-CRC-3. Figures X-CRC-1 through X-CRC-117 illustrate the intended standards for improvement.
- 3.1 Exceptions to these standards may be allowed subject to topography, environmental constraints, available right of way, safety considerations, and as follows:
- a. General elements of a gateway intersection are illustrated in Figures X-CRC-1 and X-CRC-7. Establish specific requirements through design.
 - b. Elements of the Main Street cross section may be modified to accommodate Light Rail Transit alignment.
- 3.2 When developing Boulevard improvements, the County should develop and implement a strategy to minimize adverse impacts to adjacent businesses.
- 4.0 New public and private streets should be designed to accommodate future development.
- 5.0 Encourage retention and development of a local street network as shown on Map X-CRC-4, and as otherwise required in the Clackamas Regional Center Design Plan.
- 6.0 Require new streets to connect uses within a development and to adjacent property, when applicable.
- 7.0 Allow new buildings to be oriented to private streets when these streets include sidewalks or raised walking surfaces, curbs, pedestrian scale street lighting and street trees.

IXVIII. PARKS, PLAZAS, CIVIC SPACES, OPEN SPACE, PATHS AND LINKAGES

- 1.0 Add parks and enhance open space to meet community needs in the general locations shown on Map X-CRC-3. Coordinate park and open space efforts with the North Clackamas Parks and Recreation District. Provide additional parks as follows:
- *Golf Course Area Park*
 - *Windmill Area Park*
 - *Northeast Area Park*
 - *Fuller Area Park*
 - *Springwater Area Park*
 - *Overland Area Park*
 - *Bell Area Park*
 - *Causey Area Park*
 - *Price-Fuller Area Park*

- 2.0 Provide plazas at the general locations shown on Map X-CRC-3, as well as at major transit stops and stations, in high intensity pedestrian areas, and near major employment facilities.
- 3.0 Provide off-street pedestrian linkages at key locations to connect residential areas, parks, and major employment areas and attractions.
- 4.0 Protect natural features such as wetlands, forested areas and riparian habitat.
- 5.0 Conduct a feasibility study of the need for a multipurpose community/cultural facility. The study should be coordinated with the County Tourism Development Council and area business groups.

IX. PHILLIPS CREEK GREENWAY

- 1.0 Work with the North Clackamas Park District, public agencies, the private sector and the community to implement the Phillips Creek Greenway Framework Plan, adopted by reference.

XI. URBAN DESIGN STANDARDS

- 1.0 Urban design standards shall be implemented to meet the goals of the Clackamas Regional Center Design Plan through standards in the Zoning and Development Ordinance.
 - 1.1 All new buildings in the Clackamas Regional Center shall be oriented to existing or new private or public streets.
 - 1.2 Maximum front yard setbacks with pedestrian amenities are required in the Regional Center to further develop a high quality pedestrian environment.
 - 1.3 Buildings on corner lots are encouraged to have entrances at the corner.
 - 1.4 When feasible and practical, buildings shall be placed to allow future infill and intensification of the site.
 - 1.5 Pedestrian amenities, as defined by the Zoning and Development Ordinance, may be used to satisfy specific percentages of landscape requirements.
 - 1.6 Where appropriate, the County may allow developments to utilize regional storm water facilities and/or for multiple property owners to utilize joint facilities.

- 1.7 Drive-through facilities may be prohibited, limited or conditioned to support the goal of creating high quality pedestrian environments.
- 1.8 Architectural design shall support and promote urban character.

TRANSPORTATION POLICIES

XII. ROADS AND STREETS SYSTEM POLICIES

- 1.0 Construct all roadway improvements identified in Map X-CRC-4 to maintain regional accessibility to the Regional Center and provide a network for all transportation modes that interconnects neighborhoods and districts, the Station Community, commercial areas, community centers, parks, libraries, and employment places, other major activities, off-street pedestrian linkages, regional multi-use paths, and area Greenway trails.
- 2.0 Street Connectivity Policies
 - 2.1 Develop a block and grid street network that serves all transportation modes with short and direct public right-of-way routes.
 - 2.2 In all new developments adjacent to corridor arterial streets, require public or private street, or private driveway connections to provide traffic flow parallel to the arterial.
 - 2.3 On major arterial streets, encourage public or private street connections at intervals of no more than 660'. Encourage more frequent public or private connections on other streets, especially those in areas planned for mixed-use or dense development.
 - 2.4 To reduce the number of local trips using 82nd Avenue, require and develop local street and commercial driveway connections on the east side of 82nd Avenue from Causey Blvd. to Otty Road. These public or private connections shall be open to public access, and may be indirect if appropriate direct routes are not feasible. This policy applies to all land use, transportation and development permits.
- 3.0 Require public local streets, private streets, and driveway connections between developments to provide public access and circulation between land uses and reduce local trips on collectors and arterials. This policy applies to all land use, transportation and development permits.
- 4.0 In the Station Community, a network of public and private streets, including arterial, collector and local streets, will provide excellent connectivity and

pedestrian access to support transit access and utilization. Generally blocks will be no more than 450 feet in length.

54.0 Congestion Performance Standards for portions of 82nd Avenue, and Sunnyside Road, and Johnson Creek Boulevard located within the Regional Center boundary or Fuller Road Station Community boundary (consistent with Metro Regional Transportation Plan standards for Centers, such as Station Communities) shall be as follows:

CONGESTION PERFORMANCE STANDARDS (Level of Service)

	Preferred Operating Standard	Acceptable Operating Standard	Exceeds Deficiency Threshold
Mid-Day one-hour	C or better	E	F or worse
Peak two-hour	E first hour E second hour	F first hour E second hour	F first hour F second hour

65.0 Congestion Performance Standards for portions of 82nd Avenue, Sunnyside Road, and Johnson Creek Boulevard located within the Clackamas Regional Center Design Plan Area and outside the Regional Center boundary or the Fuller Road Station Community boundary shall be as follows:

CONGESTION PERFORMANCE STANDARDS (Level of Service)

	Preferred Operating Standard	Acceptable Operating Standard	Exceeds Deficiency Threshold
Mid-Day one-hour	C or better	D	E or worse
Peak two-hour	E first hour D second hour	E first hour E second hour	F first hour E second hour

76.0 Monitor transportation conditions in the SE 82nd Avenue Corridor to determine if Comprehensive Plan strategies are contributing to the attainment of congestion performance standards as identified in Policies 45.0 and 56.0 above.

87.0 Provide for roadway and infrastructure improvements sufficient to support minimum planned development intensity and density.

87.1 The Regional Center Plan includes transportation and infrastructure planning that identifies certain needed roadway and infrastructure improvements necessary to support future development in the Regional Center.

87.2 These improvements, in conjunction with frontage improvements normally and legally exacted concurrent with development, are sufficient to support the minimum planned development intensity and density within the Regional Center. Developers in the Regional Center are entitled to rely on the improvements that are listed as funded in the Five (5) Year Capital Improvement Plan, as if they are already in place when submitting a master plan at the minimum densities and for approval of each phase of a multi-phase development project.

87.3 Amendments to the Comprehensive Plan or Zoning and Development Ordinance or changes in the Comprehensive Plan Map or zoning designation for the property within the Regional Center shall not be authorized unless it is demonstrated that the improvements described in Policies 78.1 and 78.2 will remain adequate to support planned development intensity and density for the Regional Center.

XIII. TRANSIT POLICIES

- 1.0 Coordinate with Tri-Met to implement Clackamas Regional Center Design Area transit service improvements planned in the Tri-Met Primary Transit Network and Tri-Met Choices for Livability, and implement additional transit improvements identified on Map X-CRC-6.
- 2.0 Coordinate with Tri-Met, Metro, ODOT, and other agencies in funding and implementing the planned Clackamas Regional Center Design Plan Area transportation improvements identified on Map X-CRC-6.
- 3.0 Coordinate with Tri-Met to implement Light Rail Transit (LRT) service to the Clackamas Regional Center area.
- 4.0 Coordinate with Tri-Met in evaluating a fareless square for the Clackamas Regional Center Design Plan Area.
- 5.0 Coordinate with a Transportation Management Association (TMA) to develop and operate a frequent, fareless or low fare Loop Shuttle Service. A conceptual alignment for the shuttle service is indicated on Map X-CRC-6; the actual alignment is to be determined by Tri-Met and the TMA.
- 6.0 Establish park and ride lots at the periphery of the Clackamas Regional Center. Future shuttle bus routes should include stops at potential park and ride sites and employer locations.
- 7.0 To improve transit speed and the capacity of 82nd Avenue, add bus queue by-pass lanes which allow busses to by-pass auto traffic at traffic signals.

8.0 Coordinate with Tri-Met to encourage and support development of structured park-and-ride lots at high capacity transit stations. When surface parking facilities are provided, encourage Tri-Met to re-use these sites for transit-oriented development.

XIVH. PEDESTRIAN AND BIKEWAY NETWORK POLICIES

1.0 Construct all walkway and bikeway improvements identified in Maps X-CRC-3 and X-CRC-7 to provide a network connecting Clackamas Regional Center Design Plan area neighborhoods and districts with transit stops, commercial areas, community centers, parks, libraries, and employment places, other major activities, off-street pedestrian linkages, regional multi-use paths, and area greenway trails. Other local sidewalks, walkways and bikeways may be identified and developed during land use review and as part of public improvements.

2.0 In the development review process, new residential and mixed use developments within the Station Community, Corridors, and Regional Center shall encourage pedestrian and bicycle travel by:

2.1 Providing direct and convenient public right-of-way routes connecting residential uses with planned commercial uses, schools, parks, and other neighborhood facilities.

2.2 Providing bike and pedestrian connections on public easements or right-of-way when full street connections are not possible, with connection spacing no more than 330' except where topography, barriers such as freeways, railroads, or environmental constraints such as streams, rivers, slopes, or environmentally sensitive areas prevent street extension.

3.0 Sidewalks shall be constructed on all public and private streets in the Clackamas Regional Center Design Plan Area, subject to topography and environmental constraints.

XIV. TRANSPORTATION DEMAND MANAGEMENT (TDM)

1.0 Work with Clackamas Regional Center Design Area employers and businesses to develop strategies that will reduce vehicle miles traveled to decrease congestion and improve air quality. Strategies to be considered include but are not limited to the following:

1.1 Employer strategies that increase vehicle occupancy, encourage work trips outside peak travel times, and promote telecommuting.

- 1.2 Facilities Improvements to encourage non-auto transportation modes which include building the area bike/pedestrian network, transit preference systems that give buses advantage over other vehicles, transit and pedestrian amenities such as covered bus stops and lighting, on-site shower and dressing areas.
- 1.3 Identify County resources and incentives needed to promote and develop TDM programs for 82nd Avenue employers, and monitor the performance of 82nd Avenue corridor TDM programs conducted by employers.
- 2.0 Develop a Transportation Management Association (TMA) with businesses within the Regional Center Design Plan Area and Tri-Met to manage TDM strategies and operate a Loop Shuttle Service.
- 3.0 Work with employers and businesses within the Regional Center Boundary and other targeted TDM areas to initiate a Transportation Management Association (TMA) to manage area TDM strategies and operate a Loop Shuttle Service.

XVI. ACCESS MANAGEMENT

- 1.0 Implement the following access management standards on 82nd Avenue within the Clackamas Regional Center Design Plan Area.
 - 1.1 Consolidate driveways/accesses to the targets shown on Map X-CRC-8.
 - 1.2 Reduce signal spacing requirements from 1,320' to 500', contingent on maintaining adequate signal progression.
 - 1.3 Coordinate with ODOT to reassess 82nd Ave. Access Management Standards if the balance of efficient traffic flow with local access needs change as adjacent land uses develop to the Corridor and Boulevard Designs.
- 2.0 Develop Clackamas Regional Center Design Area Access Management Standards for the other areas of the Clackamas Regional Center Design Plan Area that:
 - 2.1 Require driveway/access spacing to support the County functional classification of the road.
 - 2.2 Require new driveways/accesses to line up with driveways/accesses or public streets on the opposite side of the Corridor to promote safety and efficient access and egress.

- 2.3 Encourage shared driveways/accesses with adjacent properties to meet minimum driveway access spacing standard that support the functional classification of the road.
 - 2.4 Encourage connecting driveways/accesses with adjacent properties.
 - 2.5 Require developments to provide rear access to public streets whenever feasible.
- 3.0 Other than the new public street access identified in Map X-CRC-8, do not allow additional access on Johnson Creek Boulevard between 82nd Avenue and I-205.

XVII. PARKING STANDARDS

- 1.0 Encourage more efficient land use, promote non-auto trips and improve air quality within the Clackamas Regional Center Design Plan Area by establishing, by zoning, minimum and maximum parking ratios.
- 2.0 Encourage parking on all local and collector street classifications to provide a buffer between pedestrians and vehicle traffic, and provide public shared parking.

HOUSING

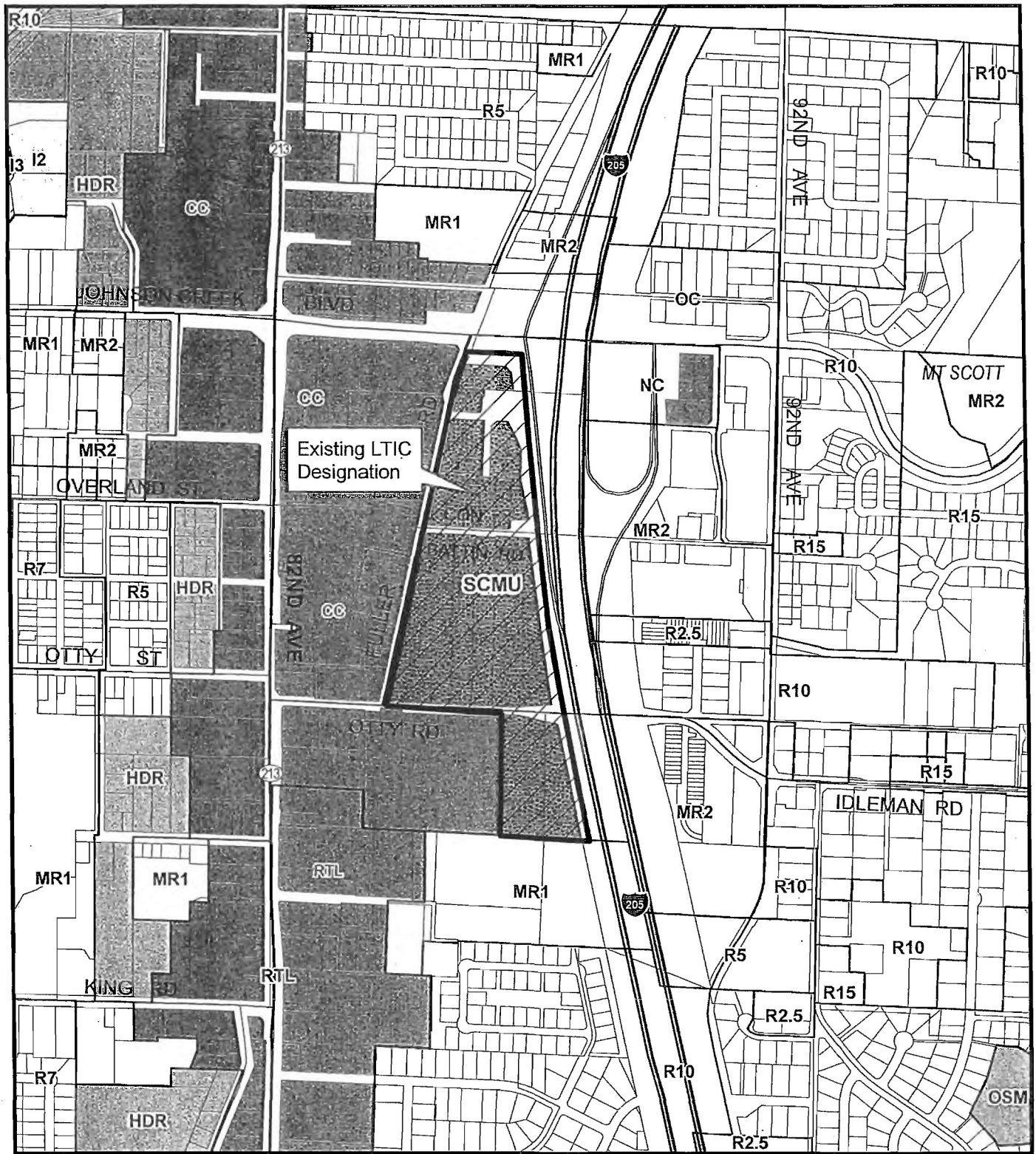
XVIII. HOUSING POLICIES

In addition to the policies in Chapter 4 of the Comprehensive Plan, the following policies apply to the Clackamas Regional Center Design Plan Area:

- 1.0 Provide for a range and variety of housing types (size and density) and variety of ownership and rental opportunities, in a range of prices.
- 2.0 Encourage housing opportunities for employees in the Clackamas Regional Center Design Plan Area by investigating partnerships to develop housing for workers in the area.
- 3.0 Limit expansion of commercial zoning into residential neighborhoods along the 82nd Avenue corridor.
- 4.0 Preserve existing mobile home parks by requiring a relocation plan to be developed and implemented by the developer for residents of mobile home parks whenever the zone designation on a mobile home park is changed to a zone other than MR-1. The County must approve the relocation plan as part of the zone change application.

- 5.0 Replace housing capacity lost in the study area by future Comprehensive Plan or zone changes. Any application for a change in Comprehensive plan designation within the Clackamas Regional Center Design Plan Area will be accompanied by a demonstration of how an equal amount of housing capacity is replaced on another site, or constructed on the site as part of a mixed use development.
 - 5.1 The purpose of this policy is to maintain the potential for the amount of housing identified in the Clackamas Regional Center Area Plan.
 - 5.2 This policy would apply to plan or zone changes made subsequent to adoption of the Clackamas Regional Center Area Plan.
 - 5.3 This policy would apply to quasi-judicial changes from residential to a non-residential use.
 - 5.4 Replacement housing capacity could be located anywhere within unincorporated Clackamas County located within the Urban Growth Boundary.
 - 5.5 Approval of a design review application and any other applicable land use permit for the required amount of replacement housing on a site in a commercial or office district, not including PMU sites, will meet the requirements of policy 5.0.

- 6.0 Form a County Housing Advisory Committee to counsel and advise the Board of County Commissioners on housing issues.
 - 6.1. Clackamas County shall review its policies and ordinances regarding affordable housing and develop an affordable housing strategy with a series of tools to provide for a mix of housing types and prices in the County.



**Ordinance No. ZDO-226,
Exhibit B**

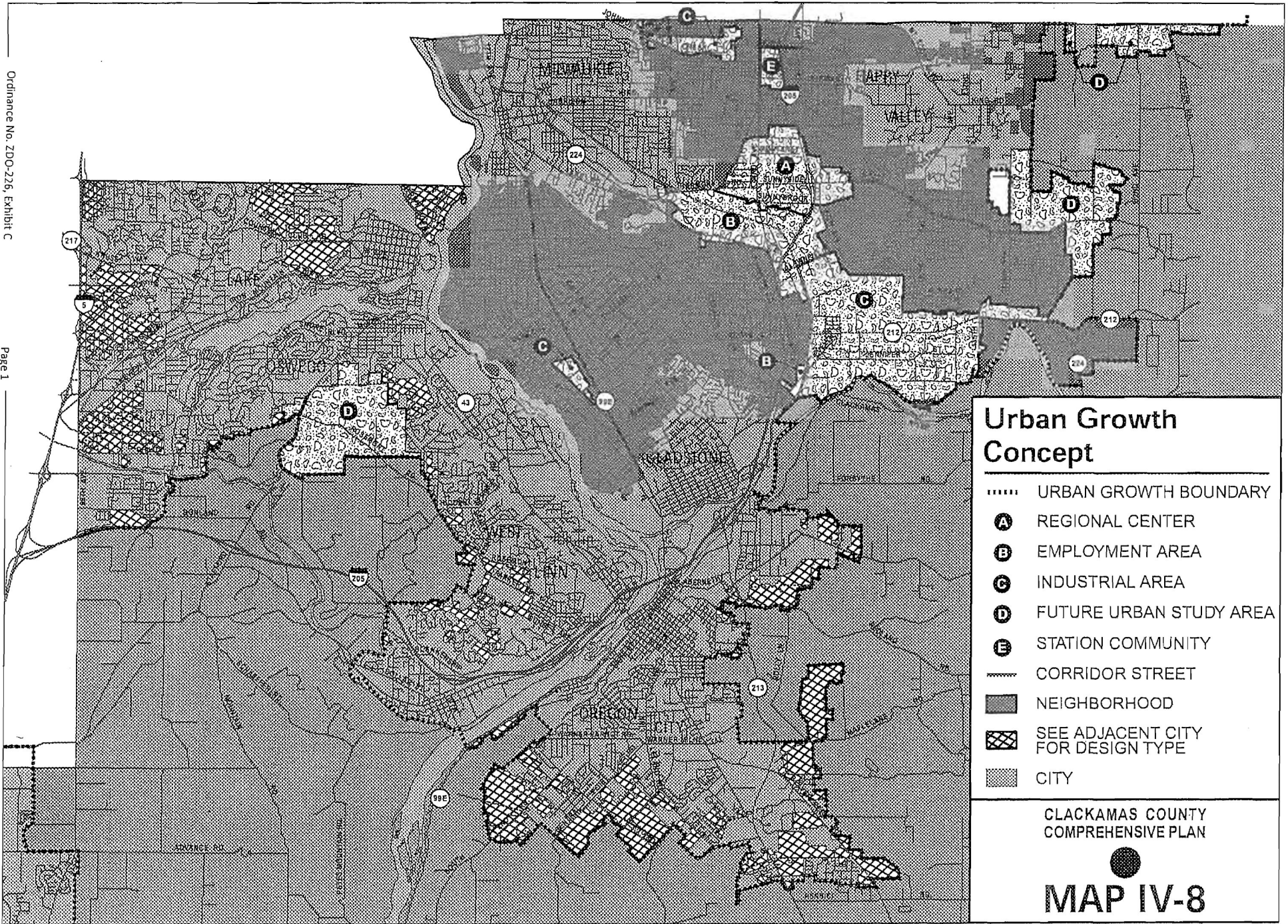


**CLACKAMAS
COUNTY**

GEOGRAPHIC INFORMATION SYSTEMS

The information on this map was derived from Clackamas County's digital GIS database. Although care was taken in the creation of this data the map is provided "as is". Clackamas County cannot accept responsibility for any errors, omissions, or positional accuracy, and therefore, no warranties accompany this product. Although information from land surveys may have been used in the creation of this product, this in no way represents or constitutes a legal land survey. Users are therefore cautioned to field verify any information before making decisions.





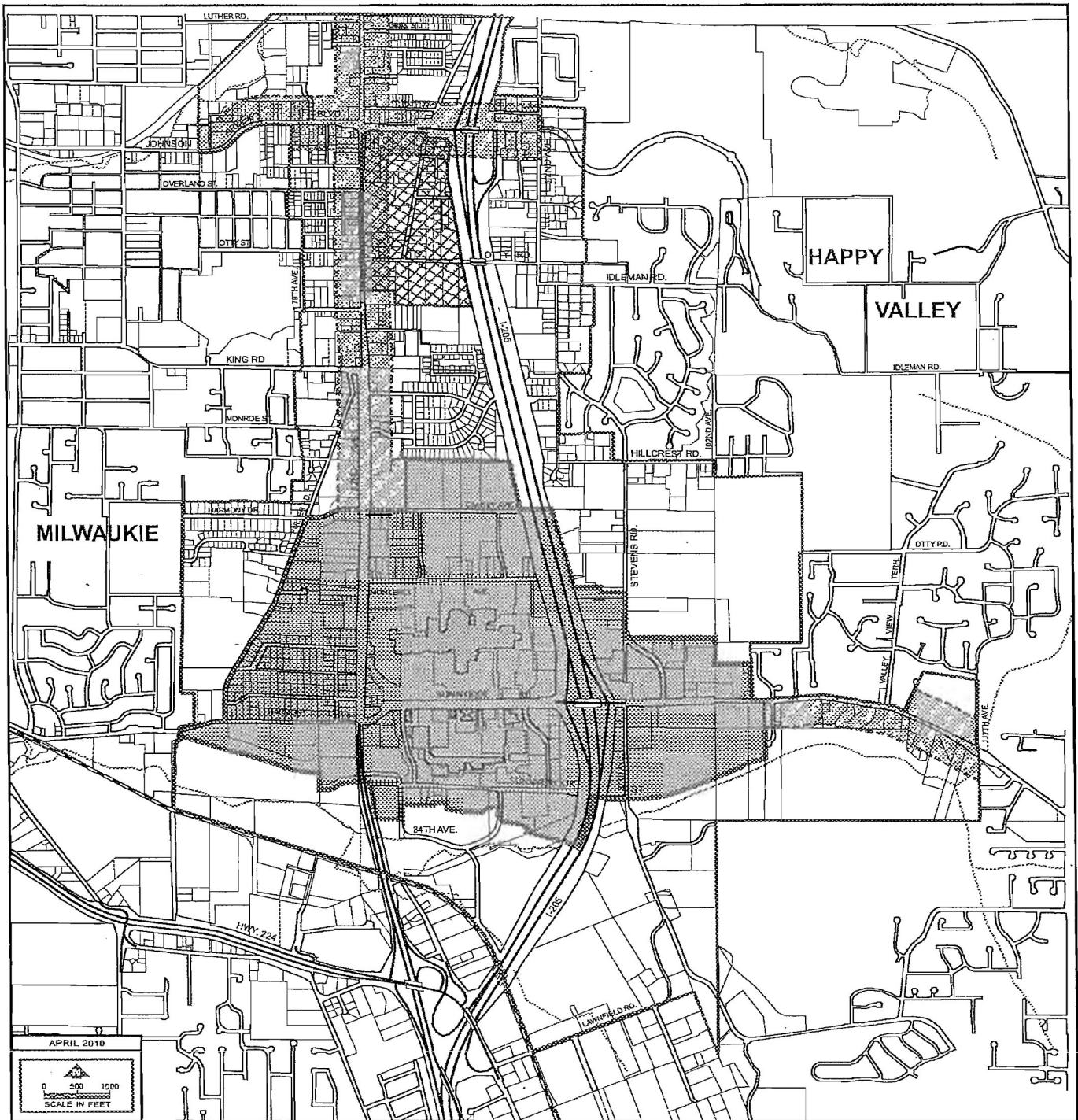
Urban Growth Concept

- URBAN GROWTH BOUNDARY
- A** REGIONAL CENTER
- B** EMPLOYMENT AREA
- C** INDUSTRIAL AREA
- D** FUTURE URBAN STUDY AREA
- E** STATION COMMUNITY
- - - - CORRIDOR STREET
- NEIGHBORHOOD
- ▨ SEE ADJACENT CITY FOR DESIGN TYPE
- ▩ CITY

CLACKAMAS COUNTY
COMPREHENSIVE PLAN

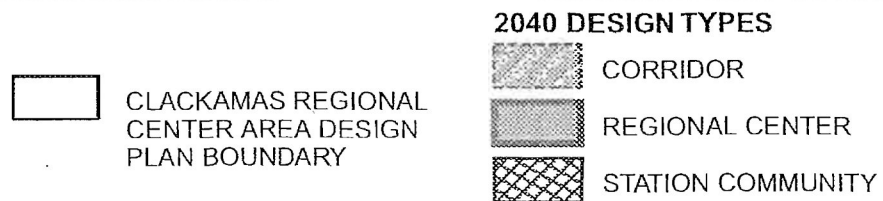
MAP IV-8

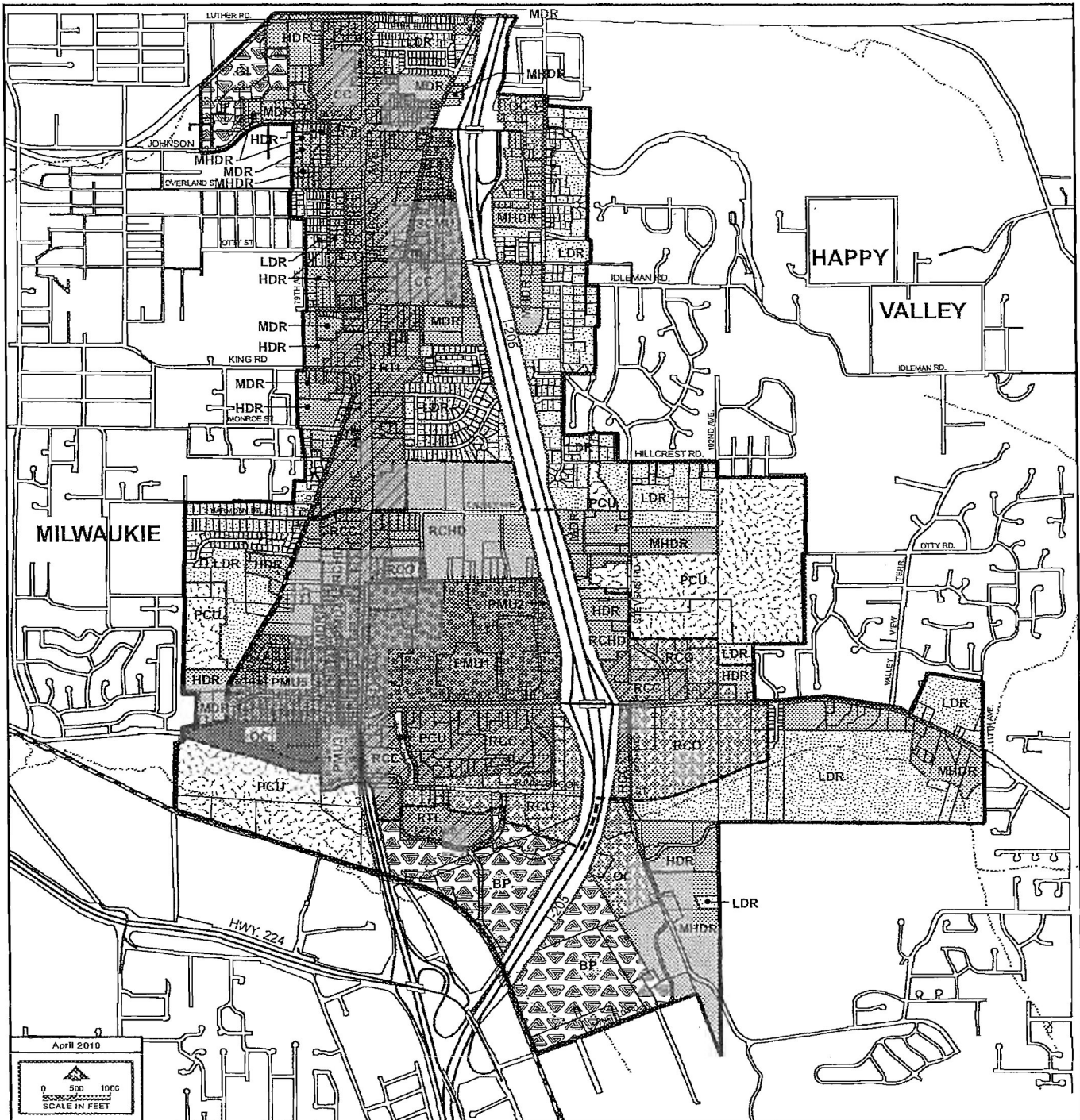
ZDO-226 2-3-2011



**Clackamas Regional Center Area Design Plan
Regional Center, Corridors and Station Community**

**CLACKAMAS COUNTY
COMPREHENSIVE PLAN
MAP X-CRC-1**



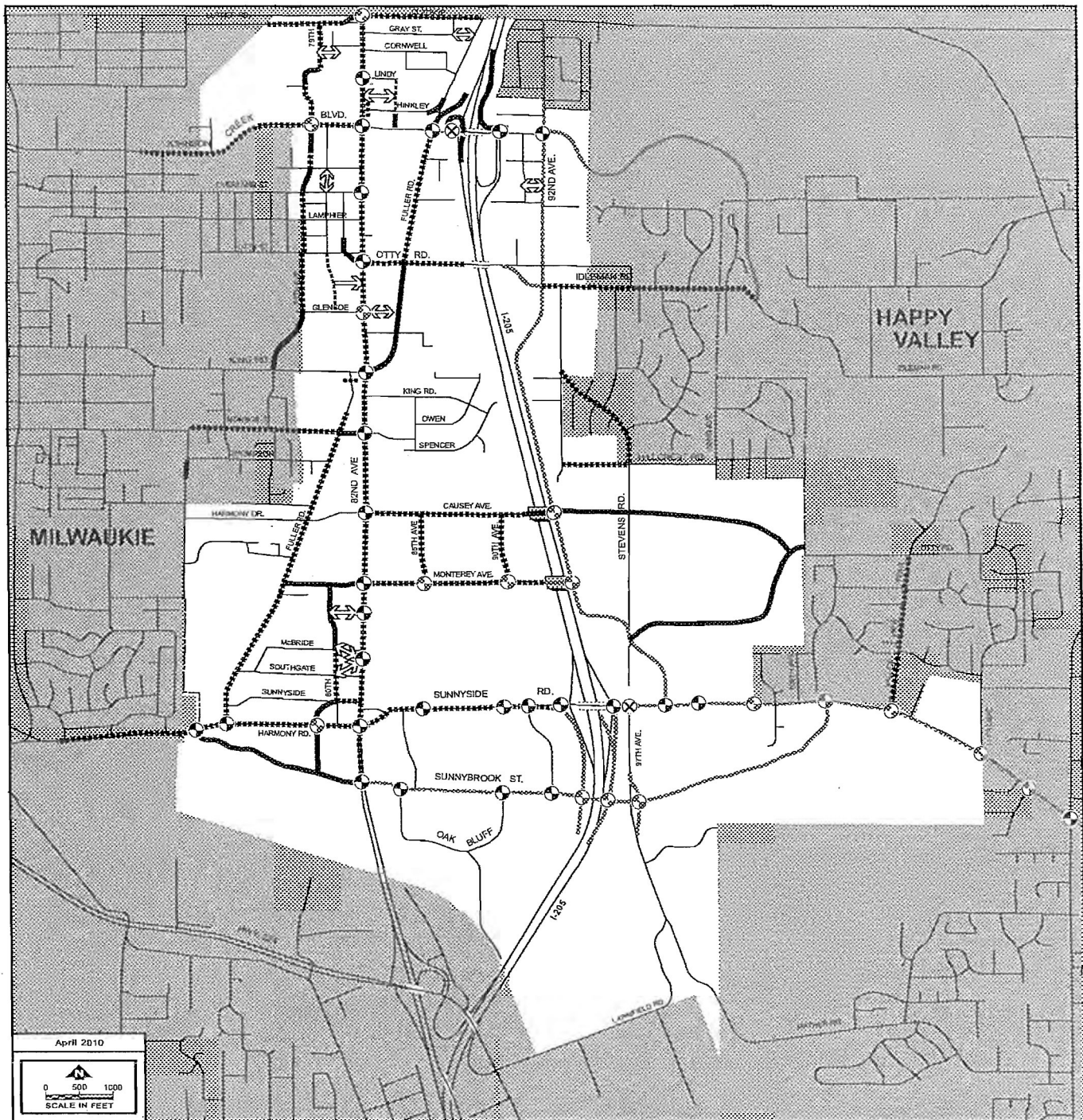


Clackamas Regional Center Area Design Plan Land Use Plan Map

CLACKAMAS COUNTY COMPREHENSIVE PLAN MAP X-CRC-2

LDR	LOW DENSITY RESIDENTIAL	RCC	REGIONAL CENTER COMMERCIAL	PMU	PLANNED MIXED USE
MDR	MEDIUM DENSITY RESIDENTIAL	RC	RETAIL COMMERCIAL	BP	BUSINESS PARK
MHDR	MEDIUM HIGH DENSITY RESIDENTIAL	CC	CORRIDOR COMMERCIAL	LI	LIGHT INDUSTRIAL
HDR	HIGH DENSITY RESIDENTIAL	RCO	REGIONAL CENTER OFFICE	GI	GENERAL INDUSTRIAL
RCHD	REGIONAL CENTER HIGH DENSITY RES.	OC	OFFICE COMMERCIAL	———	DESIGN PLAN BOUNDARY
PCU	PUBLIC AND COMMUNITY USE	SCMU	STATION COMM. MIXED USE	- - - - -	REGIONAL CENTER BOUNDARY

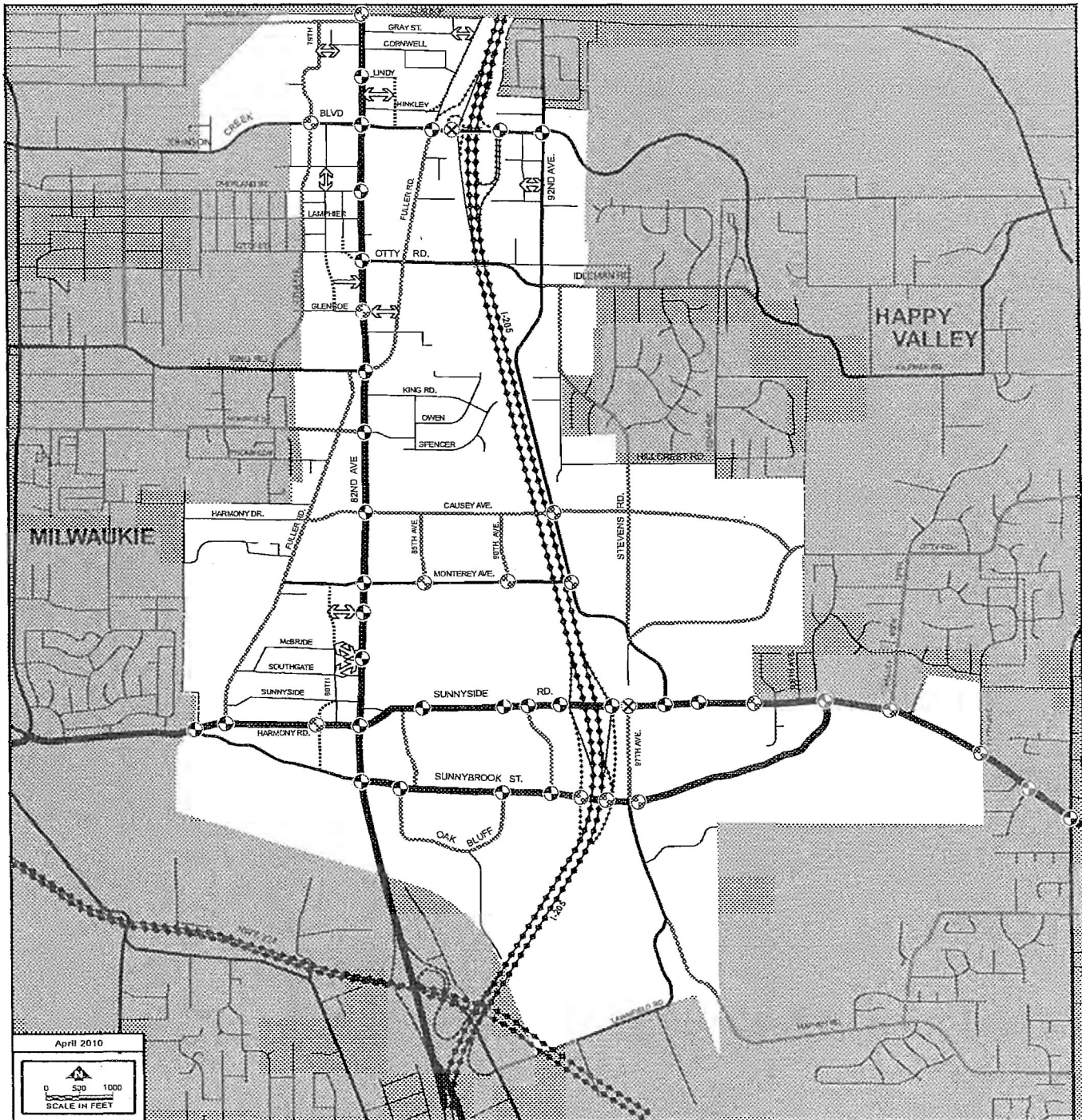
PAGEWORK/PANNING/COMPLAN/CPDWG/2010/MAP X-CRC-2(2)2010 (April 5 2010)



Clackamas Regional Center Area Design Plan Transportation Network (Public and Private)

CLACKAMAS COUNTY COMPREHENSIVE PLAN **MAP X-CRC-4**

<p>CRCA DESIGN PLAN AREA (STUDY AREA IN WHITE)</p>	<p>PROPOSED ROADS/IMPROVEMENTS</p> <ul style="list-style-type: none"> NEW ROAD (PUBLIC) ROAD IMPROVEMENT NEW LOCAL STREET LOCATION TO BE DETERMINED (PUBLIC OR PRIVATE) NEW LOCAL STREET (PUBLIC OR PRIVATE) ROAD CLOSURE 	<p>COMMITTED ROAD PROJECTS</p> <ul style="list-style-type: none"> NEW CONSTRUCTION ROAD IMPROVEMENT NEW FREEWAY OVERPASS 	<p>SIGNALIZED INTERSECTIONS</p> <ul style="list-style-type: none"> EXISTING NEW TO BE REMOVED
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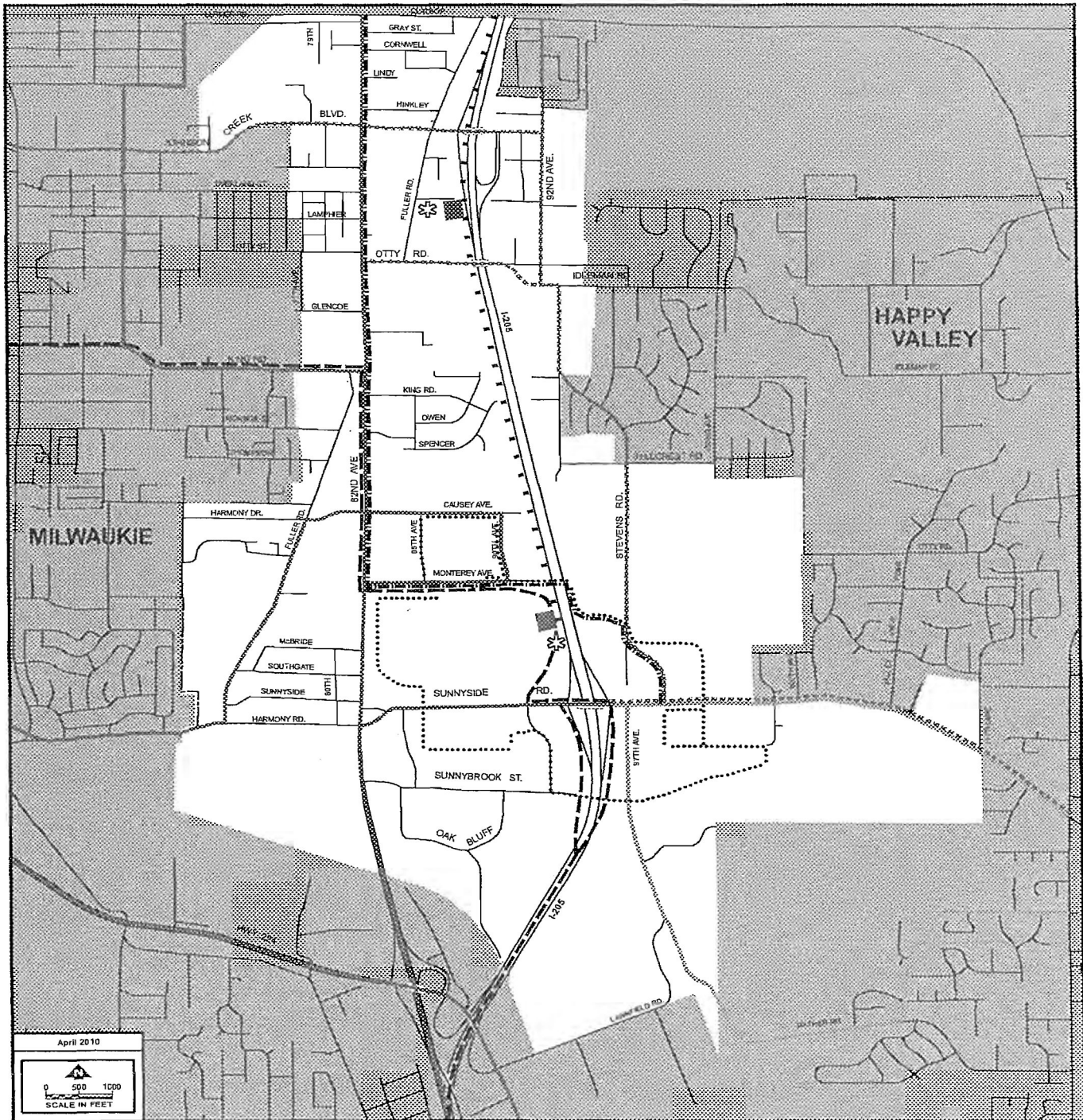


Clackamas Regional Center Area Design Plan

Functional Classification

CLACKAMAS COUNTY
COMPREHENSIVE PLAN
MAP X-CRC-5

- | | | | |
|---|----------------|--|-----------------------------------|
| CRCA DESIGN PLAN AREA (STUDY AREA IN WHITE) | FREEWAY | LOCAL STREET LOCATION TO BE DETERMINED (PUBLIC OR PRIVATE) | SIGNALIZED INTERSECTIONS EXISTING |
| FREEWAY RAMP | MAJOR ARTERIAL | LOCAL STREET (PUBLIC OR PRIVATE) | NEW |
| MINOR ARTERIAL | COLLECTOR | | TO BE REMOVED |



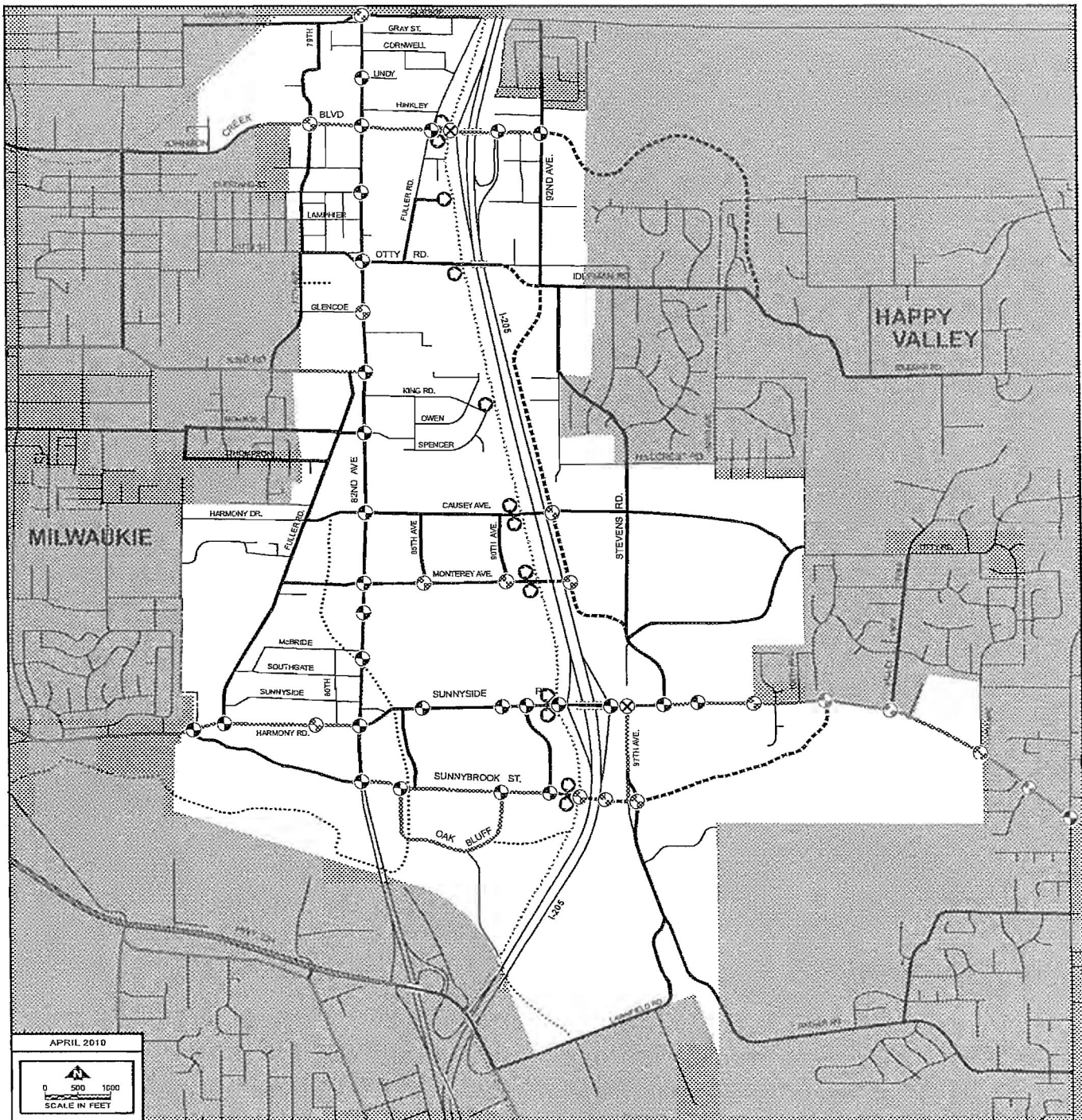
Clackamas Regional Center Area Design Plan Transit Network

CLACKAMAS COUNTY COMPREHENSIVE PLAN MAP X-CRC-6

CRC STUDY AREA
(STUDY AREA IN WHITE)

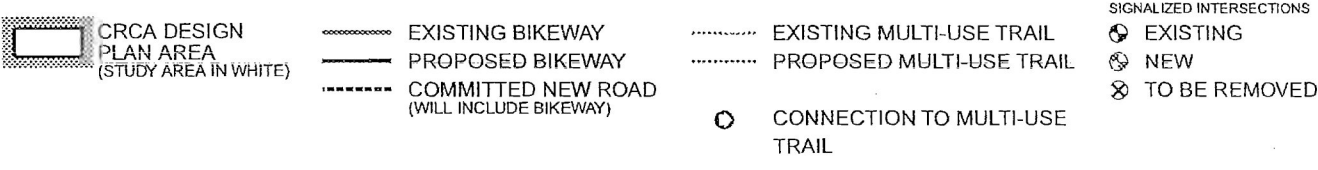
- PROPOSED ADDITIONAL TRANSIT NETWORK
- REGIONAL RAPID BUS (3-10 min.)
 - FREQUENT BUS (10 min.)
 - PRIMARY BUS (15 min.)
 - CONCEPTUAL LOOP SHUTTLE (7-10 min.)

- EXISTING TRANSIT NETWORK
- PRIMARY BUS (15 min.)
 - PEAK ONLY
 - LIGHT RAIL
 - PARK & RIDE
 - LRT STATION

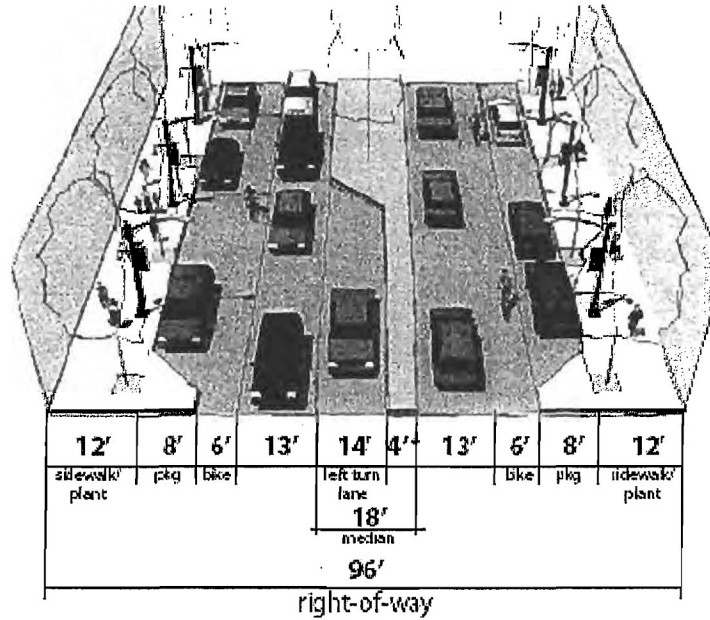


Clackamas Regional Center Area Design Plan Bikeway And Path Network

CLACKAMAS COUNTY COMPREHENSIVE PLAN MAP X-CRC-7



ZDO-226 2-3-11



NOTES

- * UTILITY EASEMENT MAY BE REQUIRED OUTSIDE OF RIGHT-OF-WAY.
- * PROVIDE LANDSCAPED MEDIAN WHERE FEASIBLE. TRAVEL LANE WIDTH 11 FT. IF NO LANDSCAPED MEDIAN

Clackamas Regional Center Area Design Plan

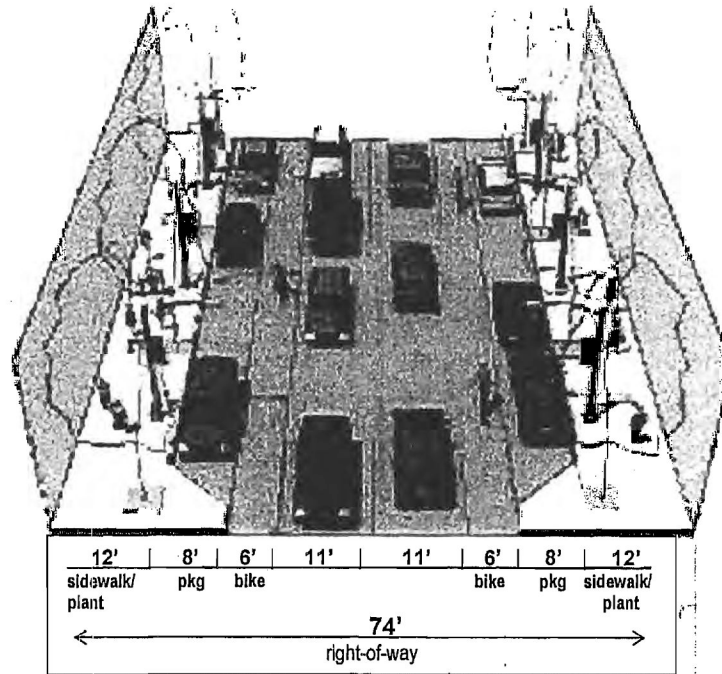
Fuller Road Station Community, Street Type "B"

Otty Road (82nd to I-205) and

Fuller Road (Otty Road north to Con Battin Road)

CLACKAMAS COUNTY
COMPREHENSIVE PLAN

FIGURE X-CRC-8



NOTES

* UTILITY EASEMENT MAY BE REQUIRED OUTSIDE OF RIGHT-OF-WAY

Clackamas Regional Center Area Design Plan

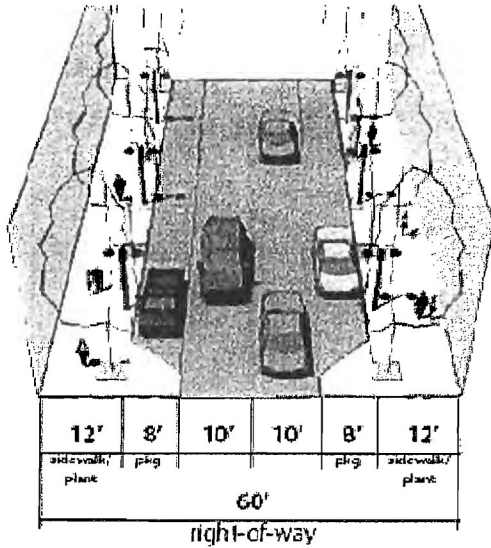
Fuller Road Station Community, Street Type "C"

*Fuller Road (Con Battin Rd north to Johnson Creek Blvd),
proposed Fuller Road (Otty Rd south), and Con Battin Road*

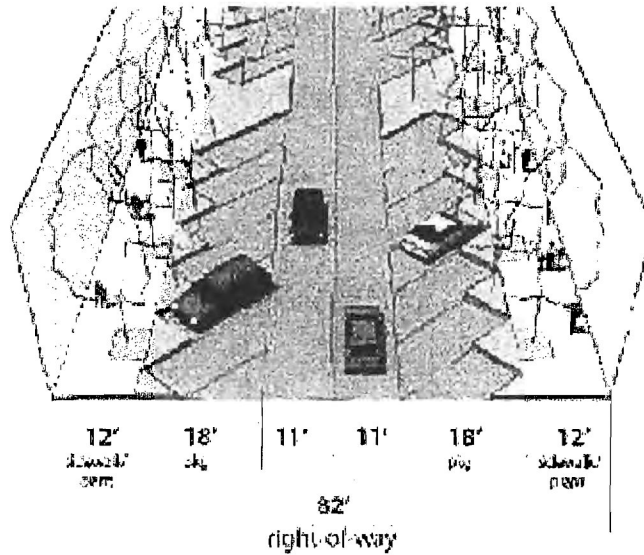
CLACKAMAS COUNTY
COMPREHENSIVE PLAN

FIGURE X-CRC-9

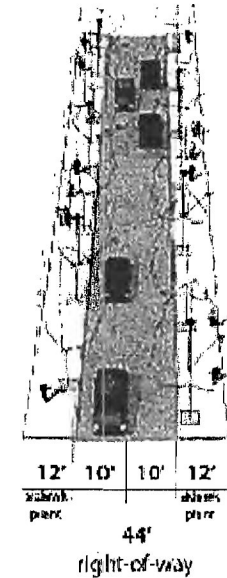
Option 1: On-street Parallel Parking



Option 2: On-street Diagonal Parking



Option 3: Narrow Street



NOTES

UTILITY EASEMENT MAY BE REQUIRED OUTSIDE OF RIGHT-OF-WAY.

Clackamas Regional Center Area Design Plan

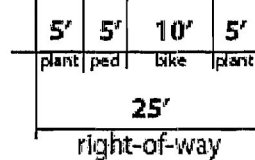
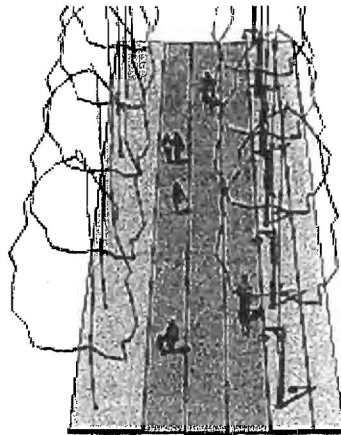
Fuller Road Station Community, Street Type "D"

Local connecting streets and Battin Road. "D" Streets may be public or private, and may take the form of private drive aisles.

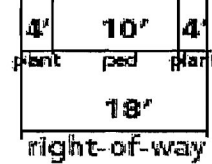
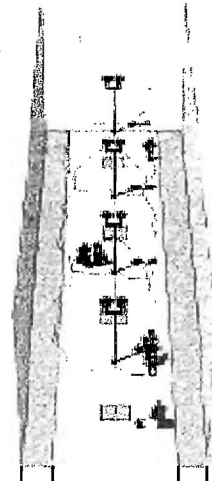
CLACKAMAS COUNTY
COMPREHENSIVE PLAN

FIGURE X-CRC-10

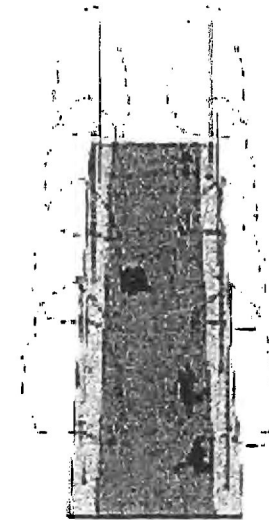
Option 1: Multi-Use Path



Option 2: Walkway



Option 3: Alley/Universal Access



18-24'
width

NOTES

UTILITY EASEMENT MAY BE
REQUIRED OUTSIDE OF
RIGHT-OF-WAY

Clackamas Regional Center Area Design Plan

**Fuller Road Station Community, Type "E"
Pedestrian/Bicycle Connection**

CLACKAMAS COUNTY
COMPREHENSIVE PLAN

FIGURE X-CRC-11

EXHIBIT E
Ordinance No. ZDO-226
Clackamas County Zoning and Development Ordinance Amendments

Text to be added is underlined. Text to be deleted is struck through.

1022 CONCURRENTNESS (~~8/18/10~~)

1022.01 PURPOSE

The purpose of Section 1022~~this section~~ is to ensure that transportation infrastructure is provided concurrent with the new development it is required to serve or within a reasonable period of time following the approval of new development. (~~8/18/10~~)

1022.02 APPLICATION OF SECTION

~~This Section 1022 shall apply to the following development applications: design review, tentative subdivision plans, tentative partition plans, and conditional uses. This section shall not apply to final plat approval for which tentative approval was granted prior to March 14, 2002, including time extensions requested under Subsections 1105.06 or 1106.05. (1/20/05)~~

1022.03 TRANSPORTATION FACILITIES

- A. Approval of a development shall be granted only if the capacity of transportation facilities is adequate or will be made adequate in a timely manner. The following shall be exempt from this requirement: (1/20/05)
1. Development that is located: (4/28/05)
 - a. In a Light Industrial, General Industrial, or Business Park zoning district; and (4/28/05)
 - b. North of the Clackamas River; and (4/28/05)
 - c. West of Highway 224 (south of Highway 212) or 152nd Drive (north of Highway 212); and (4/28/05)
 - d. South of Sunnyside Road (east of 82nd Avenue) or Harmony Road (west of 82nd Avenue) or Railroad Avenue (west of Harmony Road); and (4/28/05)
 - e. East of Interstate 205 (south of Milwaukie Expressway) or the city limits of Milwaukie (north of the Milwaukie Expressway). (4/28/05)

3. Roadways—other than 82nd Avenue and Sunnyside Road—~~within the Clackamas Regional Center, as identified on Comprehensive Plan Map X-CRC-1, Regional Center, Corridors, and Station Community boundary~~ shall be subject to the following minimums: (1/20/05)
 - a. LOS E during the weekday midday peak on-hour period; and (1/20/05)
 - b. LOS E during the first hour and LOS E during the second hour of the weekday PM peak two-hour period. (1/20/05)
 4. Except as established by Subsections 1022.03(B)(1) through (3), LOS E shall apply to developments proposed on property with a zoning designation of Campus Industrial, Light Industrial, General Industrial, Rural Industrial, or Business Park. (8/18/10)
 5. Except as established by Subsections 1022.03(B)(1) through (3), LOS E shall apply to high-employment developments. A high-employment development is one that provides a minimum of 50 FTE per acre. Only jobs where the employee reports to work at the subject property shall be included in this calculation. (8/18/10)
 6. The performance standards identified in the latest edition of the Oregon Highway Plan shall apply to facilities under the jurisdiction of the State of Oregon, with the exception of those facilities identified in Subsections 1022.03(B)(1) and (2). (8/18/10)
- C. For the purpose of calculating capacity as required by Subsections 1022.03(A) and (B), the following standards shall apply: (8/18/10)
1. Both the method of calculating LOS and the definitions given to the LOS letter designations are established by the *Clackamas County Roadway Standards*. The method of calculating capacity on state facilities is established by the Oregon Highway Plan. (1/20/05)
 2. The minimum capacity standards shall apply to all roadways and intersections within the impact area of the proposed development. The impact area shall be established by the *Clackamas County Roadway Standards*. (1/20/05)
 3. Capacity shall be evaluated for motor vehicle traffic only. (1/20/05)
 4. Except as established by Subsections 1022.03(B)(1) through (3), capacity shall be evaluated for the peak 15-minute period of both the AM weekday and PM weekday peak hours of the transportation system within the impact area. The requirement to evaluate either the

- a. At the time of land use approval, necessary improvements shall be identified and the phase for which they are necessary shall be specified. (1/20/05)
 - b. Necessary improvements for a particular phase shall either: (1/20/05)
 - i. Comply with Subsections 1022.03(D)(1) through (3) at the time of building permit approval, except that the improvements shall be scheduled to be under construction within three years of building permit approval rather than within three years of land use approval; or (08/18/10)
 - ii. Comply with Subsection 1022.03(D)(4), in which case the improvements shall be completed or guaranteed prior to issuance of a certificate of occupancy or recording of the final plat for the applicable phase. (8/18/10)
- E. As used in Subsection 1022.03(D), “necessary improvements” are: (8/18/10)
- 1. Improvements identified in a transportation impact study as being required in order to comply with the adequacy standard identified in Subsection 1022.03(B). (8/18/10)
 - a. A determination regarding whether submittal of a transportation impact study is required shall be made based on the *Clackamas County Roadway Standards*, which also establish the minimum standards to which a transportation impact study shall adhere.
 - 2. If a transportation impact study is not required, county traffic engineering or transportation planning staff shall identify necessary improvements or the applicant may opt to provide a transportation impact study.
- F. As an alternative to compliance with Subsection 1022.03(A), the applicant may make a voluntary substantial contribution to the transportation system. (8/18/10)
- 1. As used in this subsection, “substantial contribution” means construction of a roadway or intersection improvement that is all of the following:
 - a. A complete project or a segment of a roadway identified in the Clackamas County 20-Year Capital Improvement Plan (CIP), the Statewide Transportation Improvement Plan (STIP), or the

county, the applicant shall comply with the respective jurisdiction's requirements for guaranteeing completion of necessary improvements. This option is only available if the jurisdiction has a mechanism in place for providing such a guarantee. (1/20/05)

- G. Where there is a conflict between Chapter 10 of the Comprehensive Plan and Subsection 1022.03, the provisions of Chapter 10 shall take precedence. (8/18/10)

1704 CORRIDOR COMMERCIAL DISTRICT (CC) (~~12/20/07~~)

1704.01 PURPOSE

~~This Section 1704~~ is adopted to implement the policies of the Comprehensive Plan for Corridor Commercial areas. (12/21/06)

1704.02 AREA OF APPLICATION

Property may be zoned CC when the site has a Comprehensive Plan designation of Corridor Commercial and the criteria in Section 1202 are satisfied. (12/21/06)

1704.03 PRIMARY USES

The following shall be allowed as primary uses: (12/21/06)

- A. Any use permitted within the Retail Commercial District; (12/21/06)
- B. Service and retail uses where there is a need for outdoor areas in order to conduct business activities and sales or storage areas are an integral part of the use, such as lumber yards or auto sales; (12/21/06)
- C. Business Park District uses listed in Subsection 606.03(A), which are not otherwise listed as primary uses under Subsections 1704.03(A) and (B), subject to Subsection 606.03; (12/21/06)
- D. Institutional Uses: Colleges, educational institutes, private schools, commercial schools, and trade schools; art, music, and dance studios; and radio and television studios, excluding transmission towers; (12/21/06)
- E. Cultural/Public Uses: Galleries, museums, assembly or convention facilities, theaters for performing arts, exhibition halls, libraries, senior centers, and fraternal organizations; (12/21/06)
- F. High Density Residential, subject to Section 303; and (12/21/06)
- G. Wireless telecommunication facilities listed in Subsection 835.04, subject to Section 835. (3/14/02)

1704.04 ACCESSORY USES

The following shall be allowed as accessory uses: (12/21/06)

- A. Uses and structures customarily accessory and incidental to a primary use; (12/21/06)

1704.07 PROHIBITED AND PREEXISTING USES

- A. The following uses shall be prohibited: (12/21/06)
 - 1. Uses of structures and land not specifically permitted; (12/21/06)
 - 2. The use of a residential trailer or manufactured dwelling, except as an office in a commercial trailer or manufactured dwelling sales lot, unless specifically authorized pursuant to Section 1204; (12/21/06)
 - 3. New single-family and two-family dwellings; and (12/21/06)
 - 4. Retail uses larger than 60,000 square feet of gross leasable area per building or business in areas designated as Industrial on Map IV-8, Urban Growth Concept, of the Comprehensive Plan. (12/21/06)
- B. Lawfully established dwellings shall be allowed to remodel or expand without review under Section 1206. (12/21/06)
- C. A lawfully established dwelling may be converted to any use permitted in the district, subject to all requirements of this Ordinance for new development. (12/21/06)
- ~~D. No minimum lot size shall be required for a lot containing a preexisting dwelling. However, the setback and/or fire wall requirements of the Uniform Building Code shall be satisfied. (12/21/06)~~

1704.08 DIMENSIONAL STANDARDS (12/21/06)

- A. Purpose: The dimensional standards are intended to: (12/21/06)
 - 1. Provide for protection of adjacent properties; (12/21/06)
 - 2. Provide for efficient utilization of Corridor Commercial areas; (12/21/06)
 - 3. Ensure that the minimum operational requirements of the development are provided on-site; and (12/21/06)
 - 4. Site buildings to encourage and support pedestrian and transit access. (12/21/06)
- B. Minimum Front Yard Setback: 15 feet. (12/21/06)
- C. Maximum Front Yard Setback: (12/21/06)

1704.09 DEVELOPMENT STANDARDS

- A. General: Development shall be subject to the applicable provisions of Sections 1000 and 1100. (12/21/06)
- B. Clackamas Regional Center ~~Area Design Plan~~: Development within the boundary of the Clackamas Regional Center ~~Study Area~~ shown on Comprehensive Plan Map X-CRC-1, Regional Center, Corridors, and Station Community, shall be subject to the ~~Clackamas Regional Center Design Plan~~ and Section 1700. If there is a conflict between Section 1704 and Section 1700, Section 1700 shall govern. (12/21/06)
- C. Community Plans and Design Plans: Development within a Community Plan or Design Plan area identified in Chapter 10 of the Comprehensive Plan shall comply with the specific policies and standards for the adopted Community Plan or Design Plan. If there is a conflict between ~~this Section 1704~~ and a Community Plan or Design Plan, the Community Plan or Design Plan shall govern. (12/21/06)
- ~~D. Master plans: A master plan shall be required for phased development and shall be submitted for design review with the application for the first phase of development. The master plan shall demonstrate compliance with this section and Sections 1100 and 1700. (12/21/06)~~
- E.D. Improvements: The County shall require the provision of, or participation in the development of, public facility improvements to implement adopted design plans or special standards. Such improvements include, but are not limited to, the following: (12/21/06)
1. Road dedications and improvements;
 2. Signalization;
 3. Sidewalks;
 4. Crosswalks;
 5. Storm drainage facilities;
 6. Sewer and water service lines and improvements;
 7. Underground utilities;
 8. Street lights;
 9. Street trees and landscaping; (12/21/06)
 10. Parks and open space; and (12/21/06)

~~1705 LOW TRAFFIC IMPACT COMMERCIAL (LTIC) (12/20/07)~~

~~1705.01 PURPOSE~~

~~This section is adopted to implement the policies of the Comprehensive Plan for Low Traffic Impact Commercial areas. The intent of these provisions is to:~~

- ~~A. Provide for uses with low peak hour trip generation to minimize traffic impacts on adjacent streets and intersections~~
- ~~B. Provide for mixed commercial and industrial uses~~

~~1705.02 AREA OF APPLICATION~~

- ~~A. The site is designated Low Traffic Impact Commercial on the Comprehensive Plan~~
- ~~B. Areas suitable for commercial uses where existing traffic conditions preclude allowing uses which generate high peak hour traffic~~
- ~~C. Areas with visibility and access from a highway or freeway~~

~~1705.03 PRIMARY USES~~

~~A. Low Traffic Impact Commercial Uses:~~

- ~~1. Furniture stores~~
- ~~2. Auto, boat, manufactured dwelling, and recreational vehicle sales and service (11/24/99)~~
- ~~3. Mini storage~~
- ~~4. Other similar uses to the above Low Traffic Impact Commercial uses with equal or less peak hour trip generation~~

~~B. Business Park Uses~~

- ~~1. Corporate headquarters, regional headquarters, and administrative offices~~
- ~~2. Manufacturing Uses: Any manufacturing or assembly use, except primary processing of raw materials~~
- ~~3. Flex space Uses: Office, light industrial uses, including administration, direct and telephone sales, back office operations, product assembly, component and inventory warehousing, shipping~~

10. Indoor areas for display and sale of products manufactured by the same business occupying the premises, provided that the floor area of such display area constitutes no more than ten (10) percent of the floor area of the primary use, or no more than 3000 square feet, whichever is less.

11. The temporary storage within an enclosed structure of source-separated recyclable/reusable materials generated and/or used on-site prior to onsite reuse or removal by the generator or licensed or franchised collector to a user or broker.

~~1705.05 USES SUBJECT TO REVIEW BY THE PLANNING DIRECTOR (3/14/02)~~

~~The following use may be approved by the Planning Director pursuant to Subsection 1305.02: (3/14/02)~~

~~A. Wireless telecommunication facilities listed in Subsection 835.05, subject to Section 835. (3/14/02)~~

~~1705.06 CONDITIONAL USES~~

~~A. Conditional uses may be allowed subject to review by the Hearings Officer pursuant to Section 1300. Approval shall not be granted unless the proposal complies with Section 1203 and any applicable provisions of Section 800. In addition, the proposed use: (5/22/03)~~

~~1. Shall have minimal adverse impact on the appropriate development of primary uses on abutting properties and the surrounding area considering location, size, design, and operating characteristics of the use; (5/22/03)~~

~~2. Shall be located on a site occupied by a primary use, and/or in a structure that is compatible with the character and scale of uses allowed within the zoning district; and (5/22/03)~~

~~3. Shall provide vehicular and pedestrian access, circulation, parking and loading areas that are compatible with similar facilities for uses on the same site or adjacent sites. (5/22/03)~~

~~B. Uses allowed subject to the above conditions are: (5/22/03)~~

~~1. Churches, subject to Section 804; (5/22/03)~~

~~2. Hotels and associated convention facilities, gift shops, and restaurants within the hotel. (5/22/03)~~

- ~~2. Encourage the retention of large sites and their development in a coordinated and efficient manner.~~
- ~~3. Ensure that the minimum operational requirements of the development are provided onsite.~~
- ~~4. Establish the maximum limits of development.~~

~~B. General Requirements: The following dimensional standards shall apply to development of sites zoned Low-Traffic Impact Commercial:~~

- ~~1. Site Area Requirements: The minimum site area is three(3) acres. A "site area" for purposes of this section shall be the total land area to be developed as a unit, prior to the creation of any new parcels or lots within the land area. A site area may be either of the following:
 - ~~a) A single tax lot, or two or more contiguous tax lots, under the same ownership; or~~
 - ~~b) Two or more contiguous tax lots under separate ownership, provided that:
 - ~~1) All individual property owners are members of a group formed for the purposes of developing the properties as a single planned development, and~~
 - ~~2) All individual tax lot ownerships are converted into development shares, or other satisfactory arrangement, allowing all lots to be combined into one lot prior to any building permit being issued for the project.~~~~
 - ~~e) Developments may be established on a site of less than three (3) acres if the site is physically separated from all other undeveloped or underdeveloped properties in this district.~~~~
- ~~2. Minimum Front Lot Line Setback: Fifteen (15) feet. Structures on corner lots shall meet the setback on both streets.~~
- ~~3. Maximum Front Lot Line Setback: None, except: Buildings at or near a transit stop along a major transit street shall have a maximum front lot line setback of twenty (20) feet. (see diagram in Section 1005.03E3 for area of application). The setback may be from a state, county, or private street. Private streets used to meet the setback must have curbs, sidewalks or raised walking surfaces on both sides, street trees, and pedestrian-scale street lighting.~~

~~E. Master plans: A master plan is required for phased development. A Master plan shall be submitted for design review with the application for the first phase of development.~~

~~F. Street and Access Connectivity: Street connections and pedestrian and bicycle access are subject to the standards in Section 1007, and the following standards:~~

~~1. Street connections to adjacent properties may be required to increase connectivity and grid patterns that allow for future development.~~

~~2. Road and Access Easement vacations: Road vacations are prohibited in developments unless replaced with a new road or walkway that serves the same function. The replacement does not have to be in the same alignment as long as it provides access to the same areas the vacated road would have provided if constructed.~~

~~G. Parking: Parking shall be in compliance with the parking requirements listed in Section 1007.~~

~~H. Improvements: The County shall require the provision of, or participation in, the development of public facility improvements to implement adopted design plans or special standards. Such improvements include, but are not limited to, the following:~~

~~1. Road dedications and improvements;~~

~~2. Signalization;~~

~~3. Sidewalks;~~

~~4. Crosswalks;~~

~~5. Storm drainage facilities;~~

~~6. Sewer and water service lines and improvements;~~

~~7. Underground utilities;~~

~~8. Street lights;~~

~~9. Street trees, landscaping;~~

~~10. Open space and parks; and~~

~~11. The Design Elements shown on Comprehensive Plan Map X-CRC 3.
(5/3/01)~~

~~I. Manufactured Dwelling Parks: Redevelopment of a manufactured dwelling park with a different use shall require compliance with Subsection 825.03. (12/20/07)~~

1707 STATION COMMUNITY MIXED USE DISTRICT (SCMU)

1707.01 PURPOSE

Section 1707 is adopted to implement the policies of the Comprehensive Plan for Station Community Mixed Use areas.

1707.02 AREA OF APPLICATION

Property may be zoned SCMU when the site has a Comprehensive Plan designation of Station Community Mixed Use and the criteria in Section 1202 are satisfied.

1707.03 PRIMARY USES

Except where prohibited by Subsection 1707.05, the following shall be allowed as primary uses:

- A. Residential Uses: Attached single-family dwellings; three-family dwellings; multifamily dwellings; congregate housing facilities; and nursing homes; subject to Section 810;
- B. Office Uses: These uses are characterized by activities conducted in an office setting and focused on business, government, professional, health, or financial services. Examples include photocopy and mailing services; employment agencies; legal, financial, architectural, and engineering services; banks and credit unions; medical, dental, acupuncture, physical therapy, or similar clinics; and counseling services;
- C. Retail and Personal Service Uses with less than 40,000 square feet of gross leasable ground floor area per building or business: These uses involve the sale, lease or rent of new or used products to the public. They also may provide personal services, hospitality, or product repair or services for consumer and business goods. Examples include retail stores for clothing, furniture, groceries, books, home improvement and home decorating goods, and office or home business supplies; pharmacies; restaurants, cafes, and retail bakeries; and services such as travel agents, barber and beauty salons, and dry cleaners;
- D. Institutional/Civic/Cultural Uses: These uses involve activities of a public, nonprofit, or charitable nature. (For-profit ventures similar to those covered under Subsection 1707.03(D) are not necessarily prohibited in the Station Community Mixed Use District, but would have to be consistent with Subsection 1707.03(C).) They provide the service on the site or have employees at the site on a regular basis. The service is ongoing, not just for

- D. Home occupations, subject to Section 822;
- E. Family daycare providers;
- F. Self-service laundry facilities;
- G. Temporary buildings for uses incidental to construction work. Such buildings shall be removed upon completion or abandonment of the construction work;
- H. The temporary storage within an enclosed structure of source-separated recyclable/reusable materials generated and/or used on-site prior to on-site reuse or removal by the generator or licensed or franchised collector to a user or broker;
- I. Bus shelters, subject to Section 823;
- J. Park-and-ride lots;
- K. Bike racks, street furniture, plazas, drinking fountains, and other pedestrian and transit amenities;
- L. Utility carrier cabinets, subject to Section 830;
- M. Satellite dishes;
- N. Solar collection apparatus; and
- O. Parking structures.

1707.05 PROHIBITED USES

The following uses shall be prohibited:

- A. Uses of structures and land that are not within one of the primary use categories in Subsection 1707.03 or identified as an accessory use in Subsection 1707.04; and
- B. The following uses, even if part of a primary use category in Subsection 1707.03:
 - 1. Detached single-family dwellings;
 - 2. Two-family dwellings;

collection of sanitary wastes, manufacture or production of goods or energy from the biological decomposition of organic material (composting), or receiving hazardous wastes from others subject to regulations of OAR 340.100 through 110, Hazardous Waste Management. Recyclable dropoff sites are included in this category; and

17. Power production facilities, including electrical power cogeneration or production of renewable fuel resources such as alcohol, methanol, and biomass for retail or wholesale distribution.

1707.06 NONCONFORMING DWELLINGS AND CONVERSION OF DWELLINGS

- A. Dwellings classified as nonconforming uses shall be allowed to remodel or expand without review under Section 1206.
- B. A lawfully established dwelling may be converted to any use permitted in the SCMU district, subject to all requirements of this Ordinance for new development.
- C. All other lawfully established structures and uses not specifically permitted in Section 1707 shall be nonconforming uses subject to Section 1206.

1707.07 DIMENSIONAL STANDARDS

The following dimensional standards shall apply:

- A. Minimum Lot Size: New lots of record shall be a minimum of one-half acre, except that the minimum shall be 2,000 square feet for a lot developed with an attached single-family dwelling.
- B. Minimum Street Frontage: 100 feet for a new lot of record, except that the minimum for a lot developed with an attached single-family dwelling shall be 20 feet. A new lot of record with frontage on more than one street shall meet the minimum on each street.
- C. Minimum Driveway Spacing: Driveways shall be spaced no closer to one another than 35 feet, measured from the outer edge of the curb cut, unless compliance with this standard would preclude adequate access to the subject property as a result of existing off-site development or compliance with the Clackamas County Roadway Standards.
- D. Minimum Density: The minimum density for residential development shall be 20 dwelling units per net acre. Net acreage shall be determined by completing

shall be provided. The required parking space shall not be used for storing a recreational vehicle, camper, or boat.

2. For three-family and multifamily dwellings, off-street parking spaces shall be provided pursuant to Subsection 1007.07.
3. Non-residential development shall comply with the minimum and maximum parking requirements in Subsection 1007.07, provided that the total amount of parking provided for non-residential development (either onsite or offsite) shall not exceed the parking cap, regardless of the size of the proposed development or the number of pre-existing parking spaces.
 - a. The parking cap for a development site shall be based on the gross site size.
 - b. The parking cap shall be determined as follows:

<u>Total gross acres of the development site x 67 parking spaces = Parking Cap</u>
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4. On-street parking may be counted toward compliance with the minimum number of parking spaces required pursuant to Subsection 1007.07 or Subsection 1707.08(E), subject to the following standards:
 - a. The following constitutes one on-street parking space:
 - i. Parallel parking, each 22 feet of uninterrupted curb;
 - ii. Diagonal parking (45/60 degree), each 12 feet of uninterrupted curb; or
 - iii. Perpendicular (90 degree) parking, each 12 feet of uninterrupted curb.
 - b. An on-street parking space may be counted when it is on the block face abutting the subject development, does not obstruct a required clear vision area, and does not violate any law or street standard.
 - c. On-street parking spaces counted toward meeting the parking requirements of a specific use may not be used exclusively by

2. To maintain landscaping, street furniture, storm drainage, and similar streetscape improvements developed in the public right of way.
- I. Signs: The standards in Section 1010 that apply generally in all zoning districts are applicable in the Station Community Mixed Use Zone. In addition:
1. Attached single-family dwellings and three family dwellings are subject to Subsection 1010.06(A) and each dwelling unit may have one sign.
 2. Developments of multifamily dwellings are subject to Subsection 1010.06(C).
 3. All other developments, including mixed-use developments, are subject to Subsection 1010.09, except:
 - a. Pole signs, electronic message center signs, and other changeable copy signs are prohibited.
 - b. Monument signs shall not exceed a height of six feet or an area of 60 square feet, regardless of the number of tenants.
 - c. Building signs may be projecting signs, and projecting signs are subject to the following standards:
 - i. A maximum of one projecting sign per entrance per tenant is permitted.
 - ii. A projecting sign shall project no more than four feet from the building or one-third the width of an abutting sidewalk or walkway, whichever is less. However, if there is no wall sign on the same building facade, the sign shall project no more than six feet from the building.
 - iii. A projecting sign shall not exceed 12 square feet per side, excluding the support brackets. However, if there is no wall sign on the same building facade, the sign shall not exceed 24 square feet per side, excluding the support brackets.

1700 CLACKAMAS REGIONAL CENTER AREA GENERAL PROVISIONS
(CRC) (5/3/01)

1700.01 ~~A. PURPOSE~~

~~This Section 1700 is adopted to:~~

- ~~A. Implements the policies of the Clackamas Regional Center Area Plan set forth in Chapter 10 of the Clackamas County Comprehensive Plan. The purpose of these provisions is to:~~
- ~~B1. Provide for a transition to more intense land uses;~~
- ~~C2. Create ~~d~~Districts and ~~n~~Neighborhoods;~~
- ~~D3. Provide for more efficient parking;~~
- ~~E4. Improve circulation and connections for all modes of transportation within the Clackamas Regional Center and ~~t~~Transportation ~~c~~Corridors;~~
- ~~F5. Integrate land use, transportation, and urban design to encourage transit, bicycle, and pedestrian use;~~
- ~~G6. Provide more community attractions;~~
- ~~H7. Create civic spaces;~~
- ~~I8. Protect key natural features and open space;~~
- ~~J9. Provide attractive streetscapes;~~
- ~~K10. Ensure the most efficient use of land;~~
- ~~L11. Add parks and enhance open spaces; and~~
- ~~M12. Provide a safe and pleasant environment.~~

1700.02 ~~B. APPLICABILITY AREA OF APPLICATION~~

~~The provisions of this Section 1700 applies to all development within the Clackamas Regional Center Area. This area is shown on Comprehensive Plan Map X-CRC-1, *Regional Center, Corridors, and Station Community.* (5/3/01)~~

1700.03 ~~CLACKAMAS REGIONAL CENTER AREA DESIGN AND DEVELOPMENT STANDARDS~~

~~Subsection 1700.03 applies in the Clackamas Regional Center Area, including the Regional Center and the Fuller Road Station Community, as shown on~~

appropriate techniques include bays and balconies for residential upper stories, and lower levels with awnings, canopies, trim or other similar treatments.

- 3) Blank walls should be avoided when possible by modulating buildings spaces, and using features listed in (a) and (b) above.
- b. Use colors, materials and scale, as appropriate, to visually connect building exteriors to adjoining public/civic spaces such as gateways, plazas and transit stations.
- c. Complement and incorporate the natural features and terrain of the site area to the maximum extent possible;
- d. Building design should be compatible with the physical capabilities of the site, the intended use (s) and surrounding development.
- e. Freestanding Parking Structures: freestanding parking structures located within twenty (20) feet of pedestrian facilities including public or private streets, pedestrian ways, greenways, a transit station or shelter, or plaza, shall provide a quality pedestrian environment on the façade facing the pedestrian facility. Techniques to use may include:
 - 1) provide retail or office uses on the ground floor of the parking structure facing the pedestrian facility;
 - 2) provide architectural features that enhance the first floor of parking structure adjacent to the pedestrian facility, such as building articulation, awnings, canopies, building ornamentation and art; and
 - 3) provide pedestrian amenities in the transition area between the parking structure and pedestrian facility, including landscaping, trellisses, seating areas, kiosks, water features with a sitting area, plazas, outdoor eating areas and drinking fountains.

2. Building Orientation:

- a. New Buildings: Within the Regional Center boundary shown on Comprehensive Plan Map X-CRC 1, all new buildings shall have at least one public entrance oriented to a street. Entrances may be oriented to a State, County, Public, or private street. Private streets used to meet this standards must include the elements identified in subsection 1700.02G1. (5/3/01)

bD. Parking Structures Orientation: Entrances for ground-level retail uses in parking structures located within 20twenty feet of a street shall be oriented to a street as described under Ssubsection 1700.032(JG)(1).

- ~~b. Setbacks from private streets as defined in Section 1700.02G1 shall be measured from the back edge of the sidewalk; and~~
- ~~c. Within the Regional Center boundary shown on Comprehensive Plan Map X-CRC-1, pedestrian amenities as defined in 202 and Section 1009 are required between the building and the front lot line. (5/3/01)~~

EH. LANDSCAPING Landscaping: Landscaping shall comply with Section 1009 and the following:

All new development in the Clackamas Regional Center Area is subject to the provisions of Section 1009 and the following specific standards:

1. General Provisions

1a. Landscape Area Requirement: For the purpose of satisfying the minimum requirements of this section and Sections 1009 and 1700, a 'landscaped area' may include pedestrian amenities. Up to one-third (~~1/3~~) of the landscape area requirement may be met by pedestrian amenities, as described in Subsection 1700.03(H)(2) below.

2b. Pedestrian Amenities: The following guidelines apply to pedestrian amenities used to meet minimum landscaping area requirements and the requirement for pedestrian amenities within front lot line setback areas:

a.1) Pedestrian areas include plazas, courtyards, outdoor seating areas for restaurants, pocket parks, and atriums when there is direct access for pedestrians. Pedestrian areas in front of buildings should be visible from the street;

b.2) Pedestrian areas must include planters and at least two of the following amenities for every one hundred (100) square feet of pedestrian area: lawn areas with trees and seating; awnings or other weather protection; kiosks; outdoor eating areas with seating; water features with sitting areas; and drinking fountains.

3e. Structure and Understructure - Parking Area Landscaping: Subject to ~~d~~Design ~~r~~Review approval pursuant to Section 1102, parking area landscaping requirements (internal and perimeter) may be waived for non-surface parking areas when appropriate features are provided to create a transition between the parking structure or understructure entrance and the adjoining sidewalk(s). Examples of appropriate features include, but are not limited to, landscape planters and trellises, awnings, canopies, building ornamentation, and art.

4d. Street Trees: Street trees are required along all streets, except for drive aisles in parking lots. When determining the location of street trees,

- c. ~~Street trees, pursuant to the standards in Subsection 1700.03(H)(4)1 E2d.~~
- d. ~~Pedestrian-scale lighting as defined in Section 202 of the Zoning and Development Ordinance.~~ and
- e. Private streets may also provide on-street parking and at-grade loading zones, as applicable.

2. Internal Streets

- a. Internal streets may be required to connect to adjacent properties to increase connectivity and provide grid patterns that allow for future development.
- b. Internal streets shall be designed to allow for future development when applicable.
- ~~e. Within the Regional Center boundary, internal streets and driveways are prohibited between buildings and the street to which building entrances are oriented~~
- ~~d.c. Access Management: Location of new streets and private access driveways shall be subject to current County and Oregon Department of Transportation(ODOT) standards~~
- ~~e.d. All Ddevelopment shall provide, when applicable, direct street and pedestrian connections between developments and schools, parks, open space, shopping areas, employment areas, and transit stops.~~
- ~~f.e. To provide connectivity, existing platted roads within proposed developments shall not be vacated unless similar access is provided on the site.~~

3. ~~Boulevards: The following streets are designated as Regional Boulevards, and are shown on Comprehensive Plan Map X-CRC-3, *Urban Design Elements* and are subject to the design standards in Comprehensive Plan Figures X-CRC-1 through X-CRC-4 in the Comprehensive Plan. (5/3/01)~~

- a. ~~Harmony Road;~~
- b. ~~Sunnyside Road;~~
- c. ~~Sunnybrook Boulevard; and~~
- d. ~~SE-82nd Avenue, between Causey and Sunnybrook.~~

4. ~~Mainstreets~~

~~SE Monterey and a portion of Causey shown on Comprehensive Plan Map X-CRC-3 are designated as mainstreets. Development on~~

1. Facades should be varied to provide visual interest. Examples of appropriate techniques for varying facades include: articulation, placement and design of windows and entrances, building trim, art, paint, detailing, ornamentation, and planters.
 2. Provide differentiation between ground-level spaces and upper stories when uses differ. Examples of appropriate techniques include bays and balconies for residential upper stories, and lower levels with awnings, canopies, trim, or other similar treatments.
 3. Blank walls should be avoided when possible by modulating buildings spaces, and using features listed in Subsections 1700.04(A)(1) and (2).
- B. Colors, materials, and scale shall be used, as appropriate, to visually connect building exteriors to adjoining public/civic spaces, such as gateways, plazas, and transit stations.
- C. The natural features and terrain of the site shall be complemented and incorporated to the maximum extent possible.
- D. Building design should be compatible with the physical capabilities of the site, the intended use(s), and surrounding development.
- E. Freestanding parking structures located within 20 feet of pedestrian facilities, including public or private streets, pedestrian ways, greenways, a transit station or shelter, or plaza, shall provide a quality pedestrian environment on the façade facing the pedestrian facility. Techniques to use may include:
1. Provide retail or office uses on the ground floor of the parking structure facing the pedestrian facility;
 2. Provide architectural features that enhance the first floor of parking structure adjacent to the pedestrian facility, such as building articulation, awnings, canopies, building ornamentation, and art; and
 3. Provide pedestrian amenities in the transition area between the parking structure and pedestrian facility, including landscaping, trellises, seating areas, kiosks, water features with a sitting area, plazas, outdoor eating areas and drinking fountains.
- F. New buildings shall have at least one public entrance oriented to a street. Entrances may be oriented to a State, County, public, or private street. Private streets used to meet this standard must include the elements identified in Subsection 1700.03(J)(1).
- G. Drive-through window facilities are allowed, except for Main Streets designated on Comprehensive Plan Map X-CRC-3, *Urban Design Elements*, or where otherwise limited in the underlying zoning district. However,

restoration or replacement lawfully commences within three years of the occurrence of the damage or destruction.
“Lawfully commenced” shall have the meaning given in Subsection 1206.03(B). However, if the new building has more stories than the previous building, Subsections 1700.05(B) through (M) will become applicable, if required pursuant to Subsection 1700.05(A)(1).

3. Subsections 1700.05(A)(1) and (2) apply separately to Sectors 1 and 2, meaning that compliance with Subsections 1700.05(B) through (M) will not be required in Sector 1 or 2 until that particular sector exceeds the development threshold established by Subsection 1700.05(A)(1) or (2).
4. Prior to the point at which Subsections 1700.05(B) through (M) become applicable, new development in Sectors 1 and 2 shall not be sited such that it:
 - a. Precludes establishment of the “conceptual street grid” identified on Map 1700-2, or eliminates or reduces existing elements of that grid. All streets shown on the grid are planned to be Type D.; or
 - b. Precludes establishment of a connection, with a Type D street cross section, between a signalized intersection at 82nd Avenue and a point on Fuller Road within the “access area” shown on Map 1700-2.

B. Minimum Building Height: 20 feet, measured to top of parapet or roof.

C. Minimum Side and Rear Yard Setbacks: Five feet, except a zero setback is allowed for attached structures. (See Figure 1700-1.)

D. Maximum Driveway Width: The maximum width of a curb cut for a driveway is 24 feet (not including sidewalks or landscaping) unless otherwise required by the Clackamas County Roadway Standards or applicable fire district. (See Figure 1700-1.)

E. Regulating Plan: Map 1700-1 is the regulating plan for the Fuller Road Station Community. It identifies each existing or planned street in the Fuller Road Station Community as one of four street types: Type A, B, C, or D. As established by Subsections 1700.05(G) and (L), the building frontage and landscape screening regulations for the Fuller Road Station Community are applied by street type and are thereby “keyed” to the regulating plan.

F. Streets: Street improvements are required as follows:

1. Except as set forth in Subsection 1700.05(F)(3), the locations of required new streets are shown on Map 1700-1, or will be determined pursuant to Subsection 1700.05(F)(2). New streets shown on Map

- b. In lieu of compliance with Subsection 1700.05(F)(3)(a), development shall not occur until an alternative connectivity plan is approved for Sectors 1 and 2 shown on Map 1700-1. This connectivity plan shall:
- i. Connect the on-site transportation system to the existing and planned facilities shown on Map 1700-1;
 - ii. Provide pedestrian, bicycle, and motor vehicle circulation that meets the needs of future residents and visitors;
 - iii. Emphasize pedestrian mobility and accessibility, demonstrating an effective and convenient system of pedestrian walkways leading through the subject site;
 - iv. Provide for bicycle connections and efficient motor vehicle movements through the site;
 - v. Except where precluded by existing development, existing interests in real property, natural features, or topography, provide for block faces that do not exceed 450 feet between any two streets;
 - vi. Include a minimum of three street connections to 82nd Avenue and a minimum of two street connections to Fuller Road. These connections must be Type D streets, and one must connect to Fuller Road within the "access area" shown on Map 1700-2;
 - vii. Include a phasing plan for completion of the connectivity plan based on the submitted development application or conceptual future development, as appropriate. This phasing plan shall ensure that at no point is the overall connectivity in Sectors 1 and 2 reduced and that at least one connection from 82nd Avenue to Fuller Road is constructed to a Type D street cross section in conjunction with the first phase of new development; and
 - viii. Comply with the Clackamas County Roadway Standards and the requirements of the Oregon Department of Transportation, as applicable.
- c. Once an alternative connectivity plan is approved:
- i. New development shall not be sited such that establishment of the connections identified on the connectivity plan are precluded, or existing elements of that plan are eliminated or reduced.

<u>Permitted Building Frontage Type:</u>	<u>Street Type:</u>
<u>Landscape</u>	<u>A Street</u>
<u>Linear</u>	<u>A, B, C, and D Streets</u>
<u>Forecourt</u>	<u>A, B, C, and D Streets</u>
<u>Porch/Stoop/Terrace</u>	<u>B, C, and D Streets</u>

1. Buildings, except parking structures, located wholly or partially within 40 feet of a Type A, B, C or D street are required to comply with the standards for a building frontage type permitted on the applicable street type.

2. The entire length of street frontage designated on Map 1700-1 as “building frontage required,” or “required retail opportunity area,” excluding walkway cuts with a maximum width of eight feet and driveway cuts, shall be developed with one or more buildings that comply with the standards of a building frontage type permitted on the abutting street type.
 - a. Except along Otty Road, where the building frontage requirement extends the entire length of the street, the “building frontage required” designation extends a distance of 60 feet from the street intersection, and the “required retail opportunity area” designation extends a distance of 100 feet from the street intersection. The beginning point for measurement is the outside edge of the right-of-way, or in the case of a private street, the outside edge of the improved street surface, including any landscape strip or sidewalk.

3. A minimum of 50 percent of the length of street frontage not designated as “building frontage required” or “required retail opportunity area” shall be developed with one or more buildings that comply with the standards of a building frontage type permitted on the abutting street type. The 50-percent building frontage requirement is calculated for each lot individually, rather than in the aggregate for an entire street.
 - a. If part of the street frontage is designated as “building frontage required” or “required retail opportunity area,” buildings developed pursuant to Subsection 1700.05(G)(2) may be counted toward meeting the 50-percent requirement for the entire street frontage.

- ii. A fence shall be wrought iron, steel, or a similar metal and shall be dark in color. Chain-link fences are prohibited.
 - iii. A wall shall be wood, masonry, concrete, or a combination thereof.
 - iv. A fence shall be a minimum of 20 percent transparent. The transparent portions of the fence shall be distributed along the length of the fence in a recognizable pattern (e.g., two-inch gaps alternating with eight-inch solid sections).
2. Minimum Ground Floor Height: The ground floor of the building shall measure a minimum of 15 feet from floor to ceiling.
3. Minimum Building Depth: Buildings shall be a minimum of 40 feet deep.
4. Building Entrances: Building entrances shall either be covered by an awning or canopy, or be covered by being recessed behind the front building facade. If an awning or canopy is provided, it shall have a minimum vertical clearance of eight feet and a maximum vertical clearance of 13 ½ feet. If only a recessed entry is provided, it shall be recessed behind the front facade a minimum of three feet.
5. Primary Building Entrances: Each building shall have at least one building entrance that faces the street and is directly connected to a public sidewalk by a walkway that is a minimum of five feet wide.
- a. If the entrance serves a business (other than a home occupation), the entrance must be open to the public during regular business hours.
 - b. If a fence or wall is within the front yard setback as provided in Subsection 1700.05(H)(1)(e), a pedestrian opening a minimum of five feet wide shall be provided for the walkway.
6. Windows: Transparent ground-floor windows shall be provided along a minimum of 60 percent of the ground-floor, street-facing facade area.
7. Building Materials: Exterior building materials and finishes shall be high-image, such as masonry, architecturally treated tilt-up concrete, glass, wood, or stucco. Metal siding is prohibited, except as approved through design review pursuant to Section 1102 for specific high-image materials, canopies, awnings, doors, screening for roof-mounted fixtures, and other architectural features.

2. Minimum Ground Floor Height: The ground floor of the building shall measure a minimum of 15 feet from floor to ceiling, except when the building is designed to accommodate residential uses, in which case the minimum floor-to-floor height shall be 12 feet.
3. Ground Floor Construction Type: In areas designated "required retail opportunity area" on Map 1700-1, the ground floor construction type shall meet at least the minimum requirements for a commercial use, as set forth in the current edition of the Oregon Structural Specialty Code.
4. Minimum Building Depth: In areas designated "required retail opportunity area" on Map 1700-1, buildings shall be a minimum of 40 feet deep.
5. Weather Protection: Awnings or canopies shall be provided for a minimum of 50 percent of the linear distance of the street-facing building facade and shall comply with the following:
 - a. Awnings and canopies shall project a minimum of five feet and a maximum of eight feet over the sidewalk.
 - b. Awnings and canopies shall have a minimum vertical clearance of eight feet and a maximum vertical clearance of 13 ½ feet.
6. Building Entrances: Building entrances shall either be covered by an awning or canopy, or be covered by being recessed behind the front building façade. If an awning or canopy is provided, it shall have a minimum vertical clearance of 8 feet and a maximum vertical clearance of 13 ½ feet. If only a recessed entry is provided, it shall be recessed behind the front façade a minimum of three feet.
7. Primary Building Entrances: Primary building entrances shall face the street and be a minimum of 40 percent transparent. The minimum amount of transparency is measured as a percentage of the total area of the entrance.
 - a. Primary building entrances shall open onto an abutting public sidewalk, or be directly connected to a public sidewalk by a walkway that is a minimum of five feet wide.
 - b. If the entrance serves a business (other than a home occupation), the entrance must be open to the public during regular business hours.
 - a. If a fence or wall is within the front yard setback as provided in Subsection 1700.05(I)(1)(e), a pedestrian opening a minimum of five feet wide shall be provided for the walkway.

- b. The courtyard shall not be covered.
 - c. The courtyard shall be landscaped with plants, or paved with masonry pavers or stamped concrete.
 - d. The courtyard shall span a minimum of 20 feet along the street-facing building façade and a maximum of 50 percent of the street-facing building façade. As a result, the building must have a street-facing building façade of at least 40 feet wide.
3. Incorporation of Linear Building Frontage Type: The street facing-building façade not located behind a recessed courtyard shall comply with the standards for the Linear Building Frontage Type in Subsection 1700.05(I).
4. Minimum Ground Floor Height: The ground floor of the building shall measure a minimum of 15 feet from floor to ceiling, except when the building is designed to accommodate residential uses, in which case the minimum floor-to-floor height shall be 12 feet.
5. Ground Floor Construction Type: In areas designated "required retail opportunity area" on Map 1700-1, the ground floor construction type shall meet at least the minimum requirements for a commercial use, as set forth in the current edition of the Oregon Structural Specialty Code.
6. Primary Building Entrances: Primary building entrances shall face the street or the courtyard and be a minimum of 40 percent transparent. The minimum amount of transparency is measured as a percentage of the total area of the entrance.
- a. Primary building entrances facing the street shall open onto an abutting public sidewalk, or be directly connected to a public sidewalk by a walkway that is a minimum of five feet wide.
 - b. If the entrance serves a business (other than a home occupation), the entrance must be open to the public during regular business hours.
7. Windows: Transparent ground-floor windows shall be provided along a minimum of 50 percent of the ground-floor, courtyard-facing façade area. See the Linear Building Frontage Type for window requirements for the street-facing façade.
8. Building Materials: Exterior building materials and finishes shall be high-image, such as masonry, architecturally treated tilt-up concrete, glass, wood, or stucco. Metal siding is prohibited, except as approved through design review pursuant to Section 1102 for specific high-image materials, canopies, awnings, doors, screening for roof-mounted fixtures, and other architectural features.

- i. Garbage and recycling receptacles for public use are permitted, provided that they do not exceed 35 gallons in size and are clad in stone or dark-colored metal.
 - e. Fences: Fences and walls are permitted in the front yard setback area, subject to the following standards:
 - i. The fence or wall shall be a maximum of three feet high.
 - ii. A fence shall be wrought iron, steel, or a similar metal and shall be dark in color. Chain-link fences are prohibited.
 - iii. A wall shall be wood, masonry, concrete, or a combination thereof.
 - iv. A fence shall be a minimum of 50 percent transparent. The transparent portions of the fence shall be distributed along the length of the fence in a recognizable pattern (e.g., two-inch gaps alternating with two-inch solid sections).
- 2. Entry Threshold: An entry threshold, such as a porch, stoop, terrace, patio, or light court, is required and shall comply with the following standards:
 - a. The entry threshold shall have a minimum depth of five feet from the street-facing building façade to the front of the threshold.
 - b. The entry threshold height shall be no more than six feet above finished grade. An additional threshold may be provided to access a lower level and shall be no more than five feet below finished grade.
 - c. The entry threshold may be covered by a roof no larger than the threshold.
- 3. Primary Building Entrances: Primary building entrances shall face the street and be a minimum of 10 percent transparent. The minimum amount of transparency is measured as a percentage of the total area of the entrance. Each ground-floor dwelling unit, if any, shall have an individual entrance that complies with this requirement.
- 4. Windows: Transparent windows shall be provided along a minimum of 20 percent of the street-facing façade area. Windows shall be vertically oriented, but vertical windows may be grouped together to create square or horizontally-oriented rectangular windows.

- c. The underside of the trellis portion of a Low Wall and Trellis shall be a minimum of eight feet above grade and a maximum of 13½ feet above grade.
 - d. The trellis shall be heavy timber or steel (or a similar metal) and shall consist of an open structure with no decking or awning material. The trellis shall have masonry, heavy timber, or steel (or similar metal) supporting columns spaced no more than 30 feet on center.
 - e. The low wall portion of a Low Wall and Trellis shall be a minimum of 18 inches high and a maximum of three feet high and have a minimum depth of 16 inches. The low wall shall be wood, masonry, concrete, or a combination thereof.
 - f. Surface parking and loading areas shall be set back a minimum of five feet from the Low Wall and Trellis. Low shrubs, groundcover, and climbing plants shall be provided in this setback area, in lieu of trees ordinarily required pursuant to Section 1009 for perimeter surface parking and loading area landscaping. Climbing plants shall be planted at each support column.
 - g. Openings in the Low Wall and Trellis Screening are permitted for plazas that comply with Subsection 1700.05(M).
2. Urban Fence or Wall Screening Type: Urban Fence or Wall Screening, which is permitted on all street types, shall comply with the following standards (see Figure 1700-10):
- a. The fence or wall shall be set back a maximum of five feet from the front lot line.
 - b. Any area between the back edge of the sidewalk or walkway and the fence or wall shall be paved with masonry pavers or stamped concrete.
 - c. The fence or wall shall be a minimum of two feet high and a maximum of three feet high.
 - d. A fence shall be wrought iron, steel, or a similar material and shall be dark in color. Chain-link fences are prohibited. A fence shall be a minimum of 50 percent transparent. The transparent portions of the fence shall be distributed along the

d. A three-foot-high masonry wall may be substituted for the shrub row, but the trees and groundcover plants are still required.

e. Openings in the Landscaped Setback Screening are permitted for plazas that comply with Subsection 1700.05(M).

M. Plazas: Openings in required landscape screening are permitted for plazas, subject to the following standards:

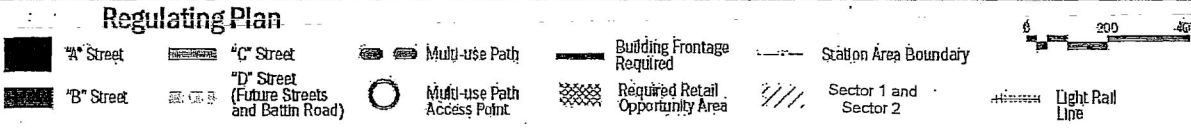
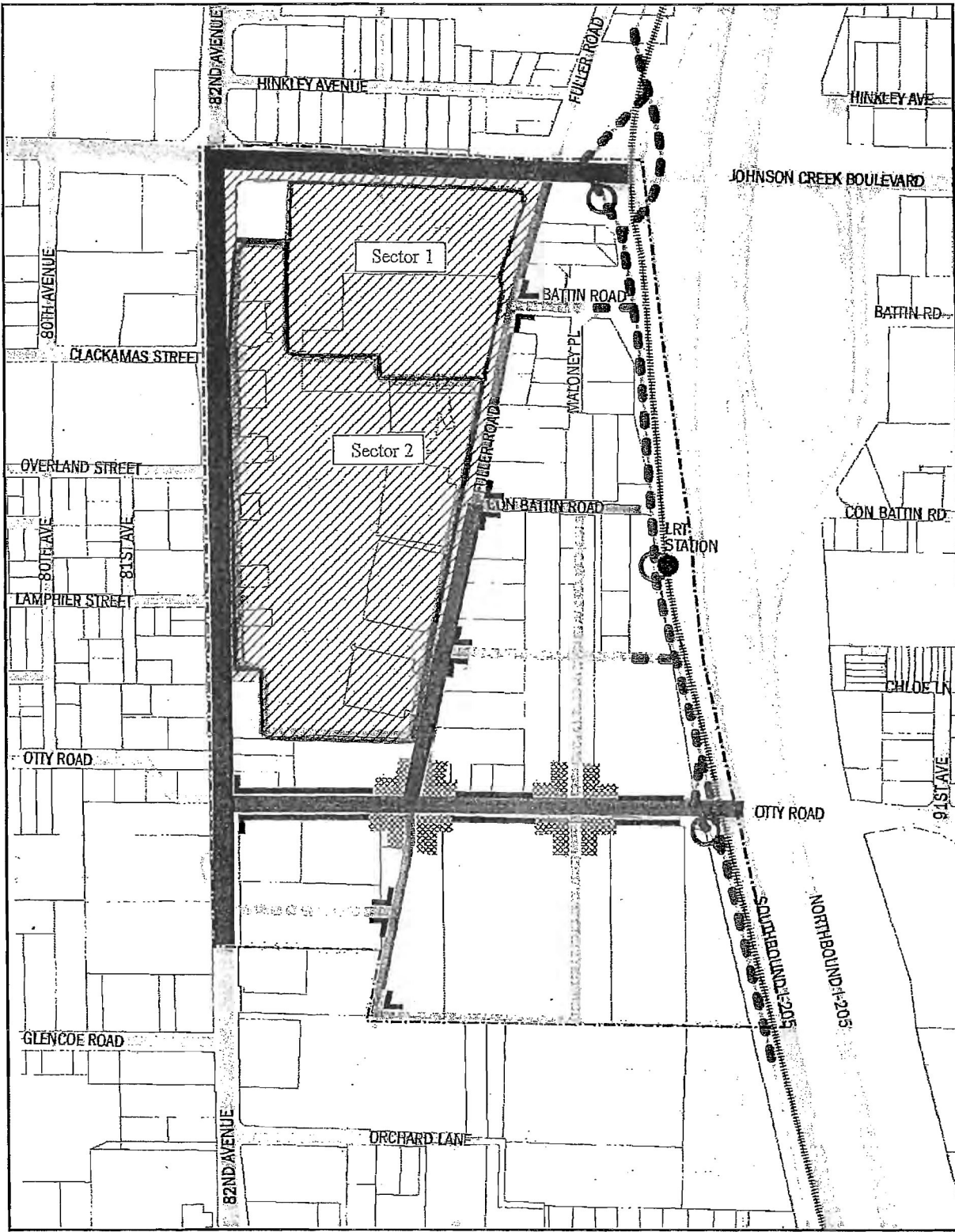
1. The plaza shall be permanent space open to the public.

2. The plaza shall be integrated in the development and be accessible from and visible from the street(s) upon which it fronts.

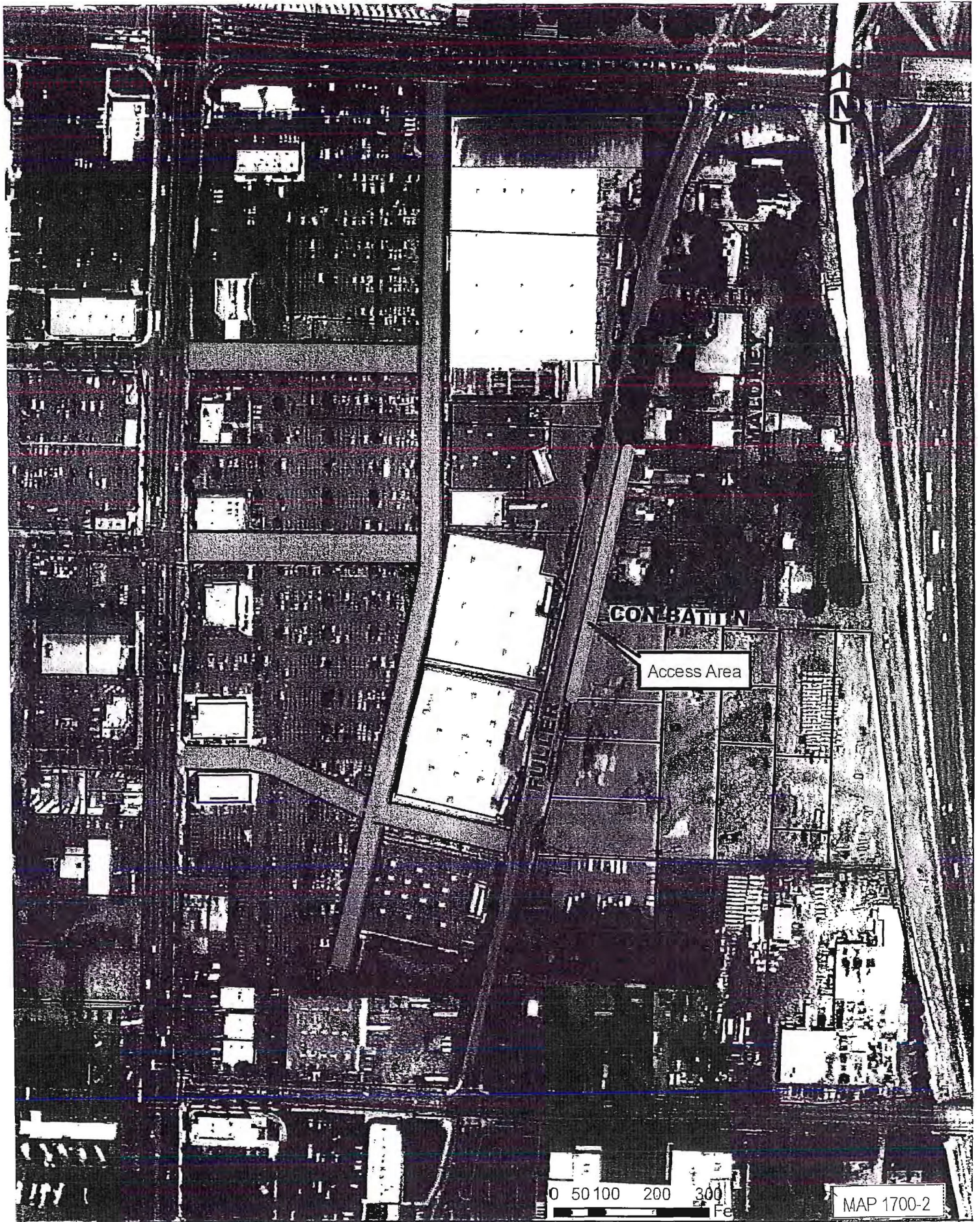
3. The plaza shall be surfaced with masonry pavers or stamped concrete.

4. Ten percent of the total plaza area shall be landscaped. Landscape planters may count toward this requirement.

5. If the plaza abuts a surface parking or loading area, it shall be separated from that area by a landscape strip that complies with Subsection 1009.04(C).



MAP 1700-1

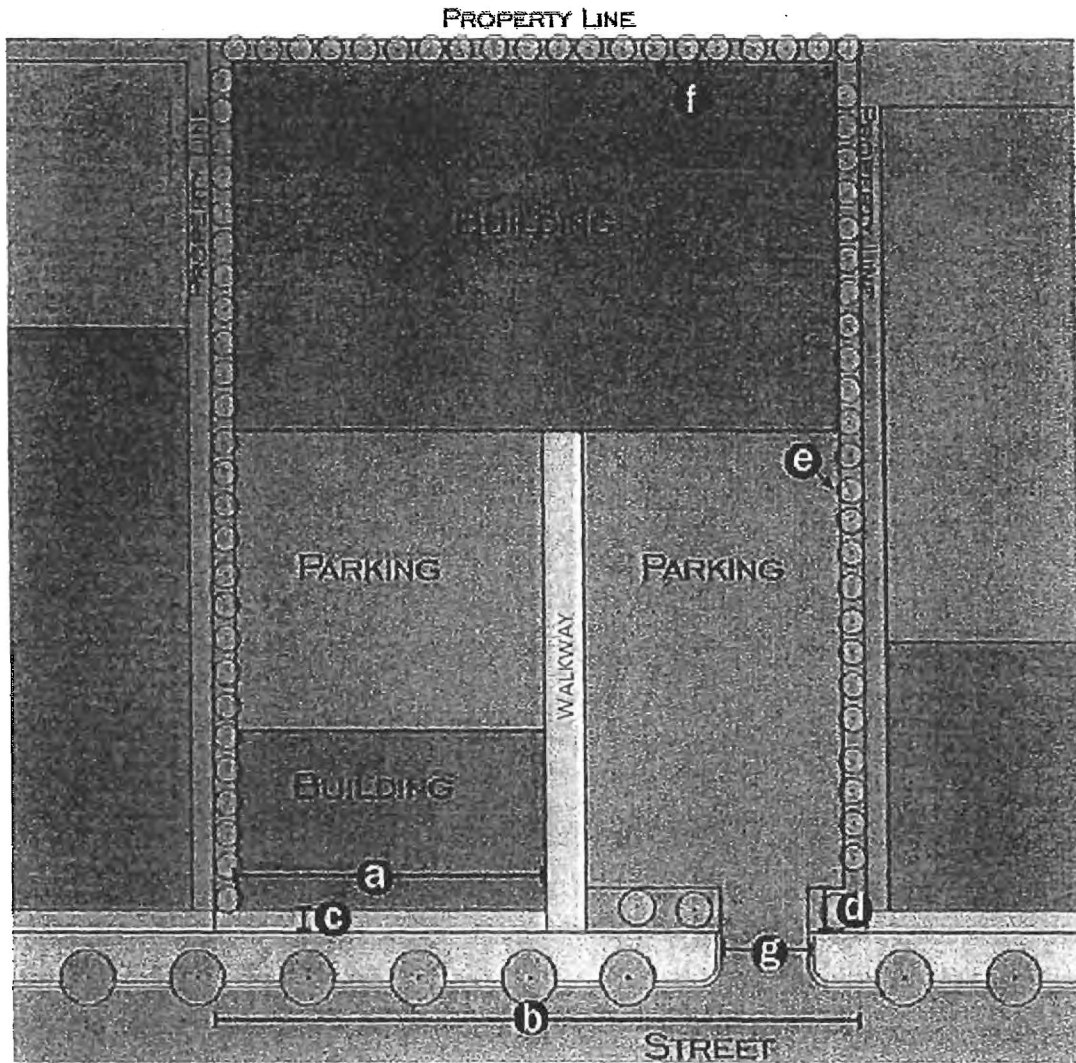


Conceptual street grid.



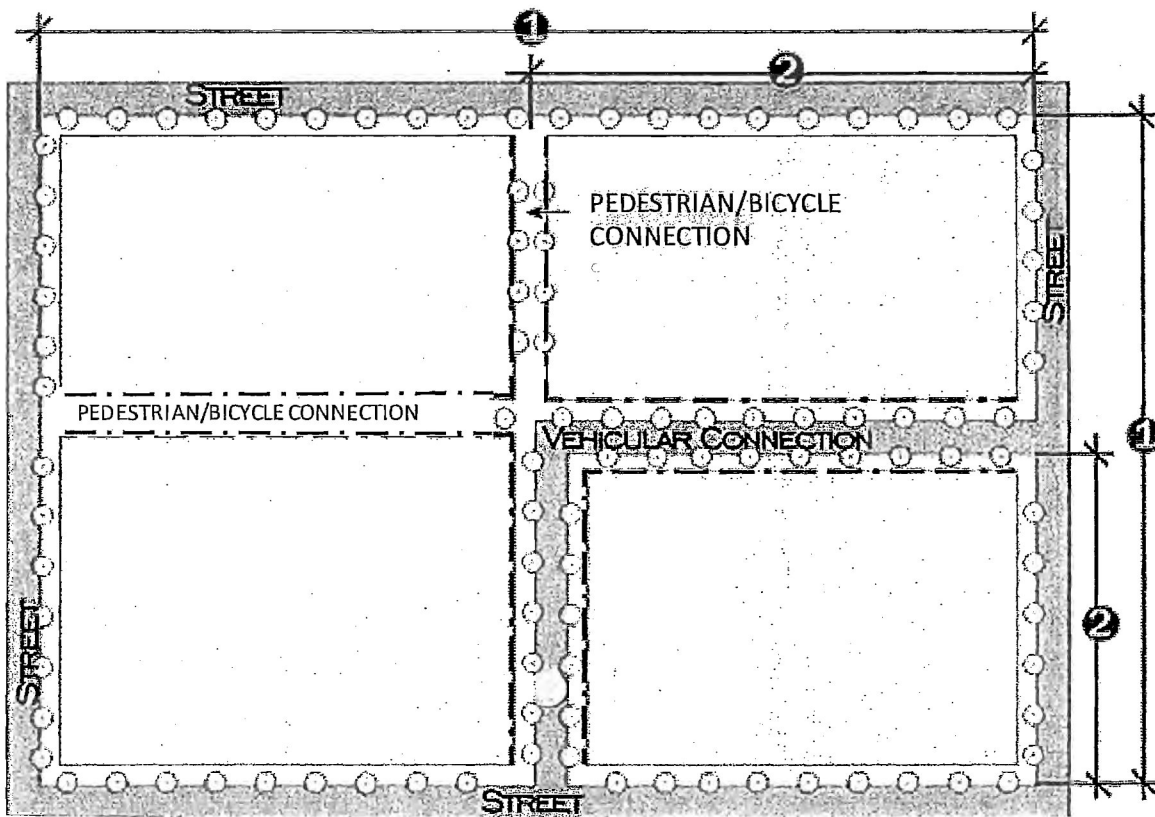
Access area.

Figure 1700-1 Illustration of Development Standards



- a** Minimum building frontage (50%)
- b** Total street frontage
- c** Required building setback from front property line (determined by Building Frontage Type)
- d** Landscape screening/front setback for surface parking areas (determined by Landscape Screening Type)
- e** Side yard setback: 5 ft. min. 0 ft. allowed for attached buildings
- f** Rear yard setback: 5 ft. min. 0 ft. allowed for attached buildings
- g** Maximum driveway width (24 ft.)

Figure 1700-2 Illustration of Through-Block Connection

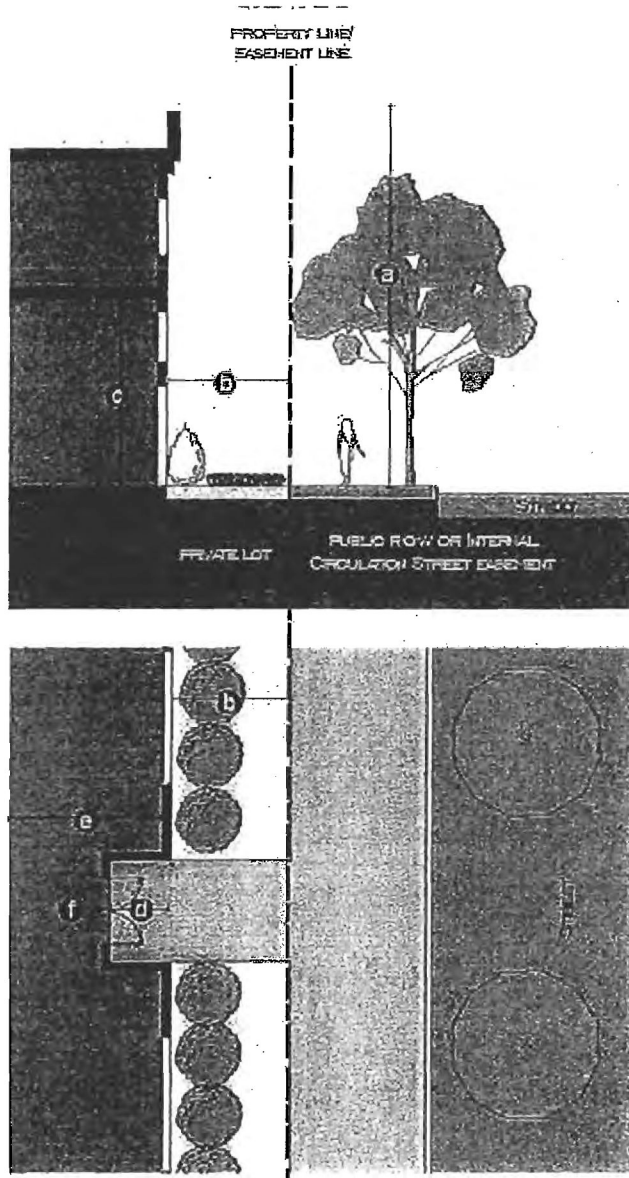


- ① Additional "D" Street or "E" Pedestrian/Bicycle connection required on block faces longer than 450 linear feet
- ② New connection no closer than 100 feet to an adjacent street intersection (existing or planned)

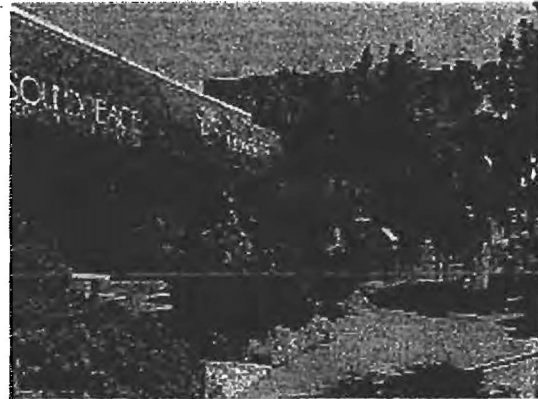
Figure 1700-3 Overview of Building Frontage Types

Building Frontage Description	Example	Plan		Section	
		Private Lot	Public ROW/ Private Rd	Private Lot	Public ROW/ Private Rd
<p>Landscape Building Frontage</p> <p>Landscape Building Frontage is set back from the front property line by a wide, landscaped strip between the building and the sidewalk. This frontage type is appropriate along streets where the existing streetscape may not be conducive to pedestrian-oriented, ground floor retail, such as where there is no on-street parking or where streets are very wide. Ground floor entries must still be provided along and connected to the sidewalk.</p>					
<p>Linear Building Frontage</p> <p>Linear Building Frontage is characterized by a façade which is built up to the property line. And the building entrance at sidewalk grade. Linear Building Frontages have substantial glazing on the ground floor and often provide awnings/ canopies over the sidewalk. Building entries must provide a canopy or awning and/or be recessed behind the front building façade.</p>					
<p>Forecourt Building Frontage</p> <p>A Forecourt Building Frontage is created by recessing a portion of the façade for a portion of the building frontage. The Forecourt Building Frontage is used in conjunction with the Linear Building Frontage, and it may be suitable for gardens and/or outdoor dining.</p>					
<p>Porch/Stoop/Terrace Building</p> <p>Porch/Stoop/Terrace Frontage is characterized by a façade which is set behind the property line and a building entry threshold, such as a porch or terrace, set between the building and the property line. The threshold may be elevated above or sunken below grade. The building entry is accessed from this threshold. Landscaping shall be provided in the setback area between the building and the sidewalk.</p>					

Figure 1700-4 Landscape Building Frontage

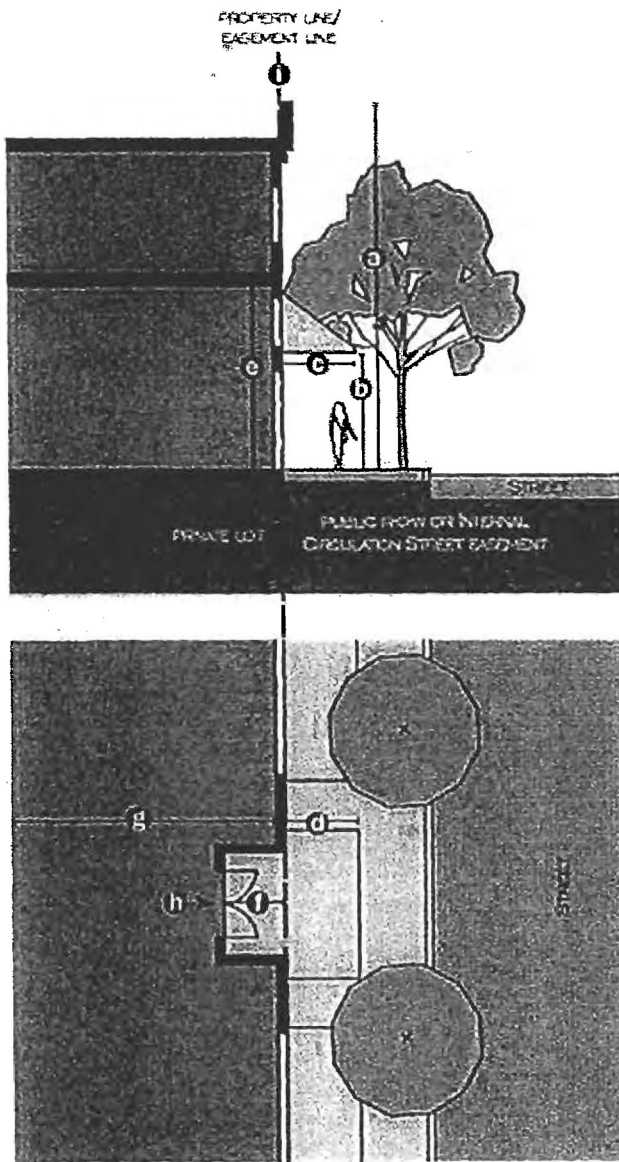


- a** Building height = 20 ft. min.
- b** Front setback = 10 ft. min/15 ft. max.
- c** Ground floor height = 15 ft. min. floor-to-ceiling.
- d** Entry recess (if no awning/canopy is present) = 3 ft. min.
- e** Building depth = 40 ft. min.
- f** Building entry (facing and directly connected to sidewalk)

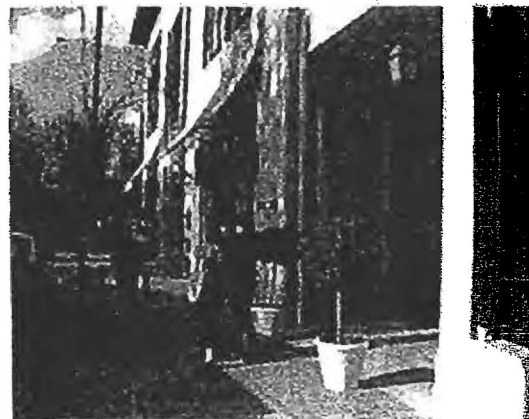


Examples of Landscape Building Frontage
Above, Retail setback with landscaping

Figure 1700-5 Linear Building Frontage

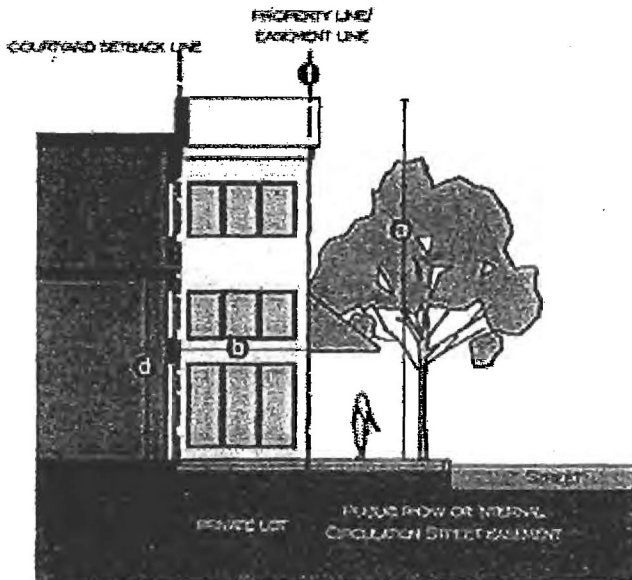


- a** Building height = 20 ft. min.
- b** Awning/canopy vertical clearance = 8 ft. min/13.5 ft. max
- c** and **f** Awning/canopy projection over sidewalk = 5 ft. min/8 ft. max.
- e** Ground floor height = 15 ft. min floor-to-ceiling or 12 ft. min floor-to-floor for residential.
- f** Entry recess (if no awning/canopy is present) = 3 ft. min
- g** Building depth = 40 ft. min.
- h** Primary entry
- i** Front setback of 0 ft. illustrated.

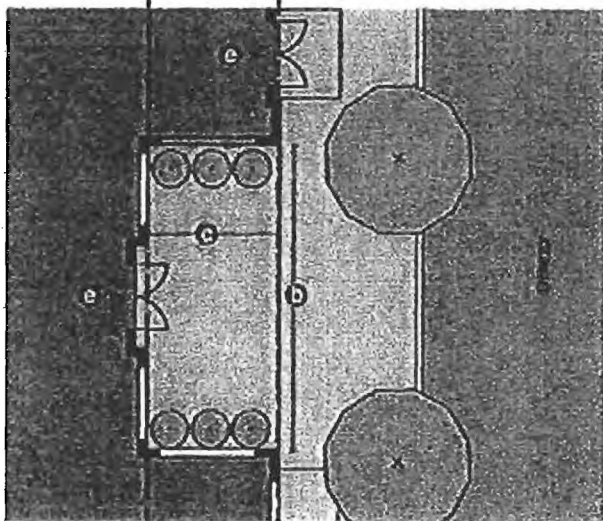


Examples of Linear Building Frontage: (Top row) Retail on ground floor with awnings; (middle row, from left) building entry with awning, glass canopy along retail storefront provides protection from rain but lets light into stores; (bottom row) canopy over retail windows with recessed building entry.

Figure 1700-6 Forecourt Building Frontage

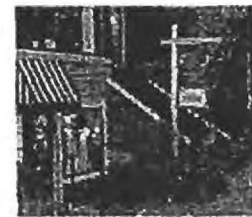
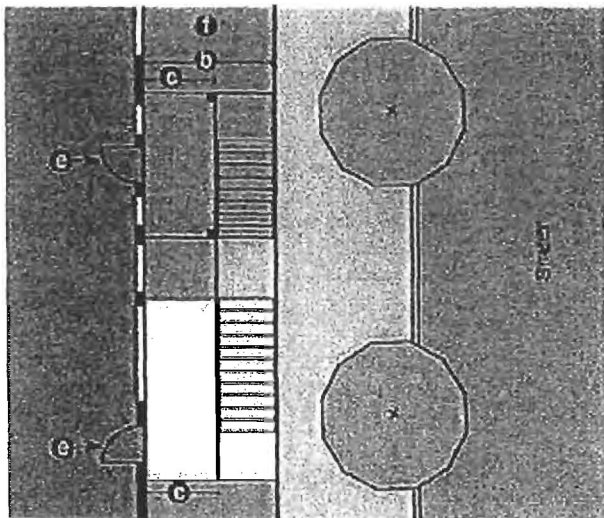
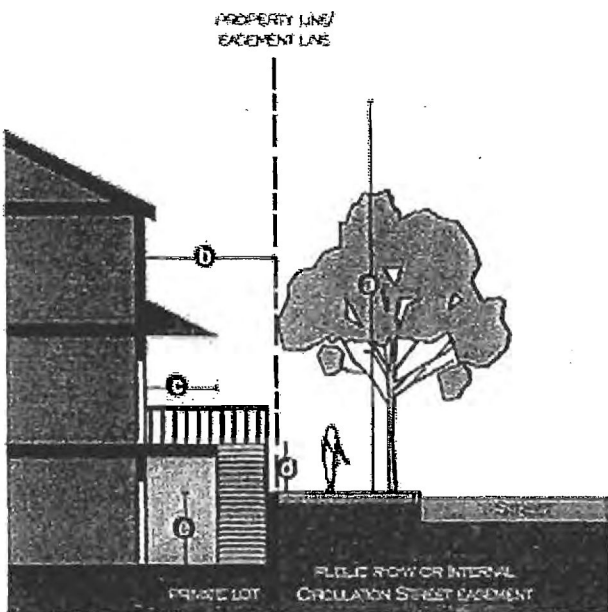


Examples of Forecourt Building Frontage : (top) Retail forecourt on local main street provides plenty of room for outdoor seating; (below) retail forecourt; (bottom) multifamily dwelling with forecourt entry.



- a** Building height = 20 ft. min.
- b** Courtyard length = 20 ft. min/50% of building frontage maximum
- c** Courtyard setback = 10 ft. min/30 ft. max.
- d** Ground floor height = 15 ft. min floor-to-ceiling or 12 ft. min. floor-to-floor for residential
- e** Primary entry
- f** Front setback of 0 ft. illustrated.

Figure 1700-7 Porch/Stoop/Terrace Building Frontage



Examples of Porch/Stoop/Terrace Building Frontage: (Top row) Ground level stoops and landscaping for townhouse units; (middle row, from left) raised terrace in front of live/work units, porch and light court combination on residence converted to retail uses; (bottom row, from left) individual porches for townhouse units, stoop entries off of street for ground floor units; (below) front stoops on townhouse units.

- a** Building height = 20 ft. min
- b** Front setback = 5 ft. min/15 ft. max
- c** Threshold depth (clear) from building façade = 5 ft. min
- d** Threshold height above grade = 6 ft. max
- e** Primary entry
- f** Required landscaping in setback



Figure 1700-8 Overview of Landscape Screening Types


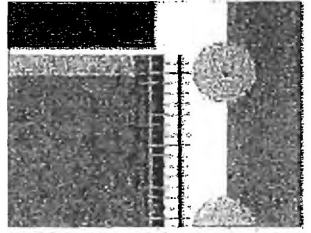
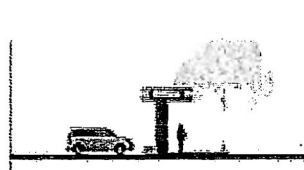

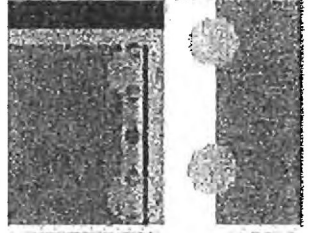


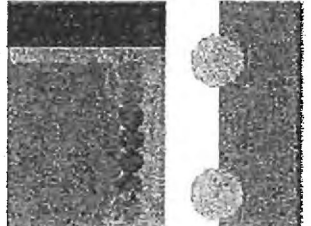
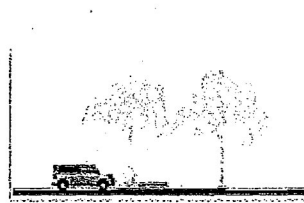
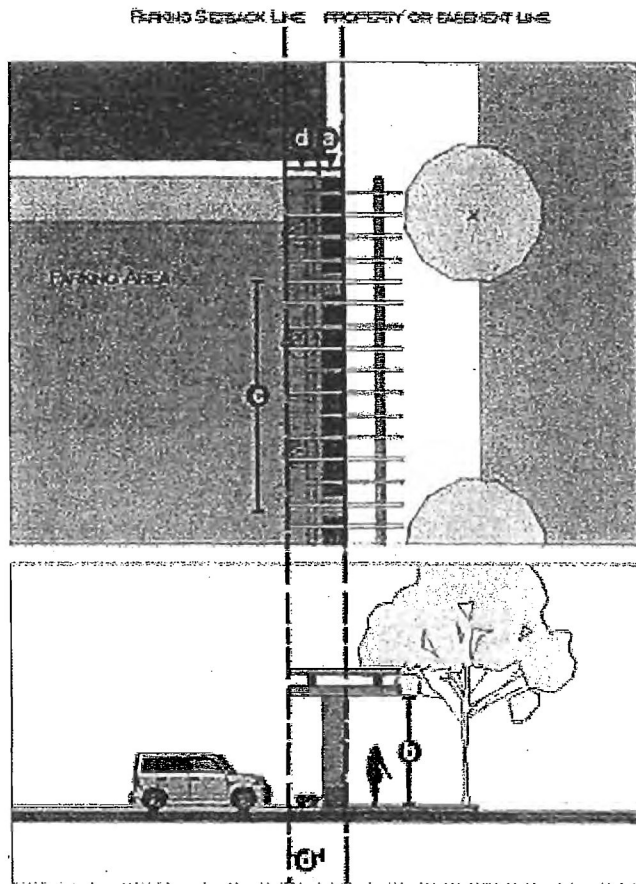
Landscape Screening Description	Example	Plan	Section
<p>Landscape Screening Type, Low Wall & Trellis Front property lines not occupied by buildings, driveways or walkways must be screened with a low wall and overhanging trellis structure.</p>			
<p>Landscape Screening Type, Urban Fence or Wall Front property lines not occupied by buildings, driveways or walkways must be screened with a fence or wall along the sidewalk with additional landscaping.</p>			
<p>Landscape Screening Type, Landscaped Setback Front property lines not occupied by buildings, driveways or walkways must have a planted landscape screen consisting of trees, shrubs (or a masonry wall) and groundcover plants.</p>			

Figure 1700-9 Low Wall and Trellis Landscape Screening

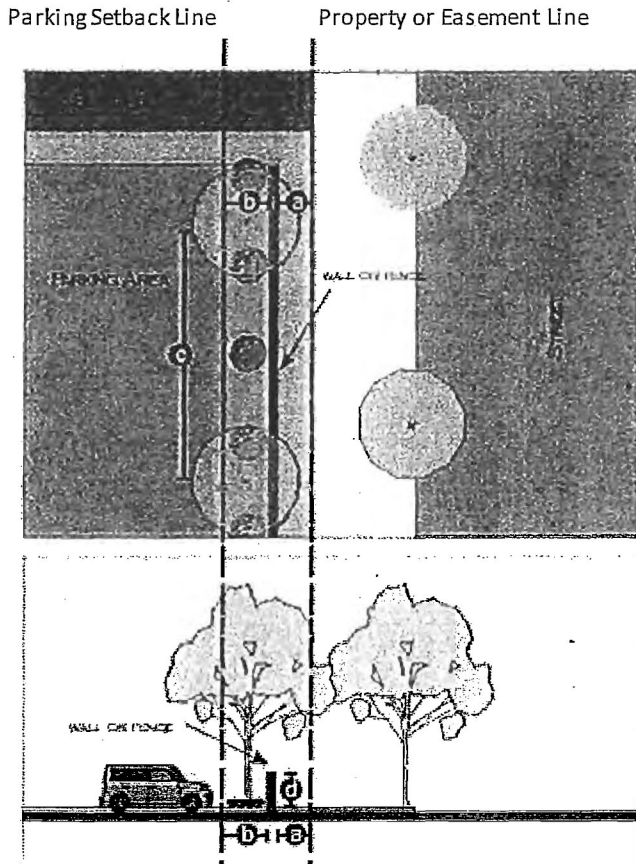


- a** Front setback = 0 ft. min/5 ft. max
- b** Height of trellis underside = 8 ft. min/13.5 ft. max
- c** Column spacing = 30 ft. max
- d** Parking area setback = 5 ft. min

Examples of Low Wall & Trellis Screening: *The trellis portion of the screen provides opportunities for increased shade and extends the feeling of a built edge along the sidewalk. The low wall provides a sense of physical separation from cars and can serve as informal seating.*



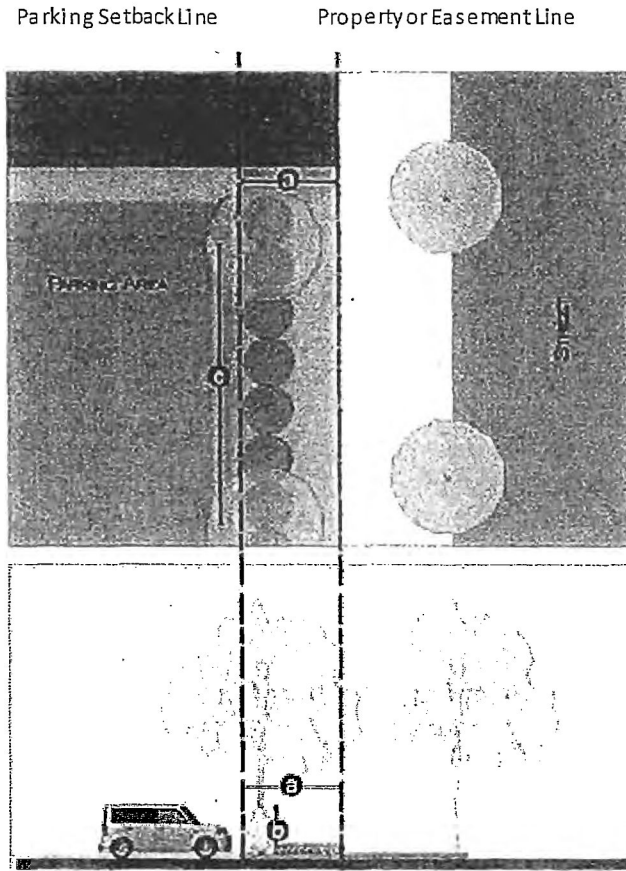
Figure 1700-10 Urban Fence or Wall Landscape Screening



Examples of Urban Fence or Wall Screening

- a** Front setback = 0 ft. min/5 ft. max
- b** Parking area setback = 5 ft. min
- c** Large tree spacing = 30 ft. max
- d** Fence height = 2 ft. min/3 ft. max
Wall height = 2 ft. min/3 ft. max

Figure 1700-11 Landscaped Setback Landscape Screening



Example of Landscaped Setback screening. Setback screening provides more room for planted landscaping.

- a** Front setback = 10 ft. min
- b** Shrub/hedge/wall height = 3 ft. min
- c** Large tree spacing = 30 ft. max



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DEPT OF
FEB 10 2011
LAND CONSERVATION
AND DEVELOPMENT

CERTIFICATE OF MAILING

I hereby certify that the enclosed Ordinance No. ZDO-226 was deposited in the mail on February 9, 2011

Signed: _____

Cheryl J. Cornelison, Administrative Assistant
Clackamas County Counsel's Office
(503) 655-8619



FIRST CLASS MAIL



CLACKAMAS
COUNTY
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OFFICE OF COUNTY COUNSEL
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DEPT.
LAND CONSERVATION
AND DEVELOPMENT

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